

# **Town of Foxfield 2014 Trail Plan**



# Acknowledgments

The 2014 Foxfield Trail Plan was funded through the Arapahoe County Open Space Grant Program. The Town of Foxfield would like to thank Arapahoe County and all of the individuals who participated in and contributed to the development of the 2014 Foxfield Trail Plan.

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The 2014 Foxfield Trail Plan was adopted by the  
Board of Trustees

On November 6, 2014

## Prepared by:

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**A RESOLUTION ADOPTING THE TOWN OF FOXFIELD TRAIL PLAN AS A  
COMPONENT OF THE TOWN OF FOXFIELD COMPREHENSIVE PLAN**

WHEREAS, pursuant to C.R.S. § 31-23-206, it is the duty of the Planning Commission to make and adopt a Comprehensive Plan for the physical development of the municipality;

WHEREAS, pursuant to C.R.S. § 31-23-207, the Comprehensive Plan shall be made with the general purposes of guiding and accomplishing a coordinated, adjusted and harmonious development of the municipality and its environs which will, in accordance with present and future needs, best promote health, safety, morals, order, convenience, prosperity and general welfare, as well as efficiency and economy in the process of development, including, among other things, adequate provision for traffic, the promotion of safety from fire, flood waters and other dangers, adequate provision for light and air, the promotion of healthful and convenient distribution of population, the promotion of good civic design and arrangement, wise and efficient expenditure of public funds, the promotion of energy conservation, and the adequate provision of public utilities and other public requirements;

WHEREAS, pursuant to C.R.S. § 31-23-208, the Planning Commission may adopt the Comprehensive Plan as a whole by a single resolution or may by successive resolutions adopt successive parts of the plan and may adopt any amendments or extensions thereof or additions thereto; and

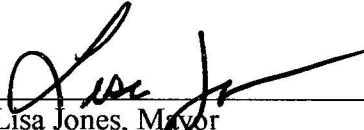
WHEREAS, the Town of Foxfield Board of Trustees, acting in its capacity as the Town of Foxfield Planning Commission, after notice and hearing of the time and place of public hearing in one newspaper of general circulation in the Town of Foxfield and in the official newspaper of Arapahoe County, held a public hearing September 4, 2014, regarding adoption of the Town of Foxfield Trail Plan as a component of the Town of Foxfield Comprehensive Plan.

NOW, THEREFORE, BE IT RESOLVED BY THE BOARD OF TRUSTEES OF THE TOWN OF FOXFIELD, COLORADO, AS FOLLOWS:

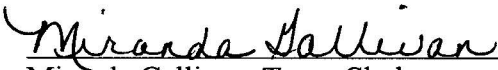
Section 1. The Board of Trustees of the Town of Foxfield, acting in its capacity as the Planning Commission of the Town of Foxfield, hereby adopts the Town of Foxfield Trail Plan attached hereto as **Exhibit 1** and incorporated herein by this reference, as a component of the Town of Foxfield Comprehensive Plan, pursuant to C.R.S. § 31-23-208, following due notice and the public hearing.

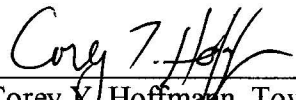
PASSED ON FIRST AND FINAL READING THIS 6<sup>th</sup> day of  
November, 2014.

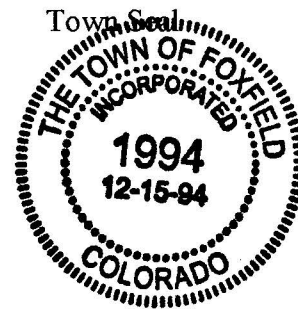
**BOARD OF TRUSTEES,  
TOWN OF FOXFIELD, COLORADO**

  
\_\_\_\_\_  
Lisa Jones, Mayor

ATTEST:

  
\_\_\_\_\_  
Miranda Gallivan, Town Clerk

  
\_\_\_\_\_  
Corey Hoffmann, Town Attorney  
(Approved as to Form)



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# Introduction

## Plan Purpose

The purpose of the 2014 Foxfield Trail Plan is to develop a framework for building an integrated system of trails that connects Foxfield to nearby destinations as well as the surrounding regional trail system, thereby providing safer access throughout the community to key public amenities and commercial areas. The future trail network will provide Foxfield residents and the regional community with opportunities for daily recreation, physical activity and improved health through active living. It will serve transportation and recreational needs and help to encourage economic growth. Building upon existing planning efforts and existing facilities, this Trails Plan identifies specific trail corridors with design recommendations and guidelines, which together will ensure a comprehensive trail network capable of serving multiple users. The Plan provides the answers to the following questions:

- How can we develop a trail system that provides access to local/nearby destinations and the surrounding regional trail system while maintaining the town's rural character?
- How will the trail facilities be designed and built?
- How long will it take to build the trail system and how much will it cost?
- How can we ensure that trail development and long-range trail maintenance/management will be adequately funded?

This Plan is not a detailed plan of trail alignments that have been field checked and designed with detail. This level of analysis/design will occur as part of the Trail Plan Implementation, set forth in the Trail Phasing and Implementation Guide. Potential connections must be subjected to an appropriate engineering feasibility analysis. This Plan is also not an acquisition plan. While it does recommend some trail corridors in areas not owned by the public, determination of the appropriate location of a specific trail and whether easements can be accommodated should be made during more detailed trail studies and after discussions with property owners along these corridors.

## Plan Background

The initial concept of a trail and open space system was first envisioned by the community in the 1998 Master Plan. A 2004 update to the Master Plan identified specific policies to guide the Town's elected and appointed officials in decisions related to the development of a trail and park system. In 2005 and 2007 the Town received grant funding from Arapahoe County Open Space to purchase two parcels that make up the existing Norfolk Open Space. A third update to the Master Plan in 2008 included the addition of the first Foxfield Trail Plan Map.

In February of 2013, the Town submitted a Planning Grant Application to the Arapahoe County Open Space Grants Program for the development of a Trail Plan that would identify a future trail system within Foxfield and key connections to the existing regional trail network, specifically the Cherry Creek Regional Trail. This Trail Plan is the result of a collaborative effort between Foxfield residents, Town Board, Town Staff, and the Trail Advisory Group (TAG) to implement the Foxfield Master Plan trails and recreation goals by establishing open space, trails and a park system appropriate to the scale and character of the Foxfield community.

## Plan Scope and Public Involvement

The 2014 Foxfield Trail Plan is founded in community participation and represents a collaborative effort between the Foxfield Town Board, Town Staff, adjacent municipalities, property owners, community organizations and residents. The planning process was divided into four specific phases: Data Gathering, Community Engagement, Drafting the Plan and Plan Adoption and Implementation. The trail planning process was based on the belief that community input and consensus are necessary in order to create a plan that appropriately reflects Foxfield's unique rural character. The Town and the Trail Planning Team gathered feedback from the community through a diverse range of methods, including administering a Trail

Plan Survey, developing a Trail Advisory Group (TAG) and hosting a Community Workshop that encouraged residents to share their opinions via keypad polling technology. Further detail regarding these aspects of the community engagement process is provided in the appendix. Residents' perspectives regarding trail planning goals and objectives, as well as more specific details related to trail use, facilities, amenities, location and construction methods, were used to inform and generate this final Trail Plan. Key findings from various public engagement events that led to the recommendations in this plan include:

- Many participants in the planning process currently use local or regional trails on at least a weekly basis, primarily for exercise and fitness.
- The majority of residents who participated in the planning process were in favor of the Town of Foxfield creating new trails, as well as improving and linking existing trails.
- Residents believe that better connectivity with regional trails, particularly the Cherry Creek system, and connectivity with commercial areas would increase trail usage throughout Foxfield.
- Residents believe that separate, dedicated walking paths and sidewalks would allow for safe passage within the town.
- Residents have some concern related to the costs of trail funding and maintenance, as well as privacy and safety related to trail development.

## Vision, Goals and Objectives

### Vision

Foxfield's trail system will provide safe, convenient and accessible facilities for various user groups including pedestrians, cyclists and equestrians while maintaining the town's rural and primarily single-family residential character. It will provide connectivity within the town and to other key destinations such as regional trails, parks, schools, retail centers and adjacent residential neighborhoods. The system will offer residents and visitors an alternative to automobile travel, promoting a healthy lifestyle and opportunities to connect with Foxfield's rural character.

### Goals and Objectives

The following goals and objectives serve to support the vision statement. They were developed based upon the input obtained from the Foxfield Trail Survey, regional stakeholders (TAG) and residents of Foxfield and adjacent neighborhoods.

#### 1.0 Safety & Accessibility

**Goal:** Trail facilities that are clean, safe, efficient and accessible.

**Objectives:**

- Design trails that accommodate a variety of users and reduce user conflicts;
- Provide corridors with high visibility from adjacent roads and private properties;
- Provide safe crossings at intersections with roadways;
- Restrict unauthorized motorized vehicle access;
- Construct trails to national standards for user safety;
- Identify and install trail safety signage and trail information, where appropriate;
- Establish safe routes to nearby schools.

## 2.0 Recreation/Fitness

**Goal:** To improve opportunities for daily recreation, physical activity and healthy lifestyles for the local and regional community.

### Objectives:

- Link primary destinations such as parks, retail centers, schools and adjacent residential neighborhoods;
- Provide areas for rest and socialization along trails;
- Incorporate trailheads at schools, parks, and other locations where parking and other facilities currently exist;
- Provide trails for people of all ages and abilities including elderly and disabled trail users;
- Provide shade and areas for rest along the trails;
- Promote health/fitness benefits of trail use.

## 3.0 Financing, Maintenance & Management

**Goal:** To adequately fund trail development and long-range trail maintenance/management.

### Objectives:

- Design trails and amenities for low maintenance and vandal resistance;
- Include future trail construction and long range maintenance costs in the Town's budget and Capital Improvements Program;
- Pursue grant funding and private donations options for the development of new trails;
- Develop a maintenance program with adequate funding to ensure that trails are inspected and maintained on a regular schedule;
- Ensure that construction and materials are of high quality to reduce long term maintenance costs.

## 4.0 Connectivity

**Goal:** To address access to local/nearby destinations and the surrounding regional trail systems.

### Objectives:

- Provide interconnected trail corridors that link parks, employment/retail centers, residential neighborhoods and schools;
- Secure trail easements based upon the general location of trail corridors shown on the Foxfield Trail Plan Map;
  - ✦ Utilize the Development Review process to acquire future trail easements on undeveloped properties;
- Encourage bicycle use on public roadways;
- Encourage developers to include trails/access in future development;
- Work with other governments, agencies and community partners to complete missing trail links within existing trail corridors.

## 5.0 Regional Cooperation

**Goal:** To maintain good working relations with other governmental jurisdictions, adjacent communities, public agencies and private organizations in the region.



**Objectives:**

- Establish and maintain excellent communication with other government officials, and continue to build on the relationships created from the Trail Advisory Group;
- Provide a copy of the Foxfield Trail Plan to adjacent municipalities and relevant agencies;
- Participate in regional trail planning efforts and coordinate trail implementation activities with Arapahoe County, City of Centennial, City of Aurora, Colorado Department of Transportation (CDOT), and the Cherry Creek Basin Working Group to accomplish mutual trail goals;
- Promote intergovernmental financing for trail projects that will benefit the regional community;
- Support regional efforts to connect pedestrian and bicycle routes to neighboring communities and the regional trail networks;
- Develop partnerships and user agreements with utility companies to develop trail corridors within existing utility easements, where safe and appropriate;
- Work with CDOT and City of Aurora on trail improvements outside of the Town's municipal boundary.

**6.0 Design**

**Goal:** Trail design will meet the needs of the community.

**Objectives:**

- Provide trails for a variety of user groups including runners, walkers, strollers, bicyclists, hikers, skaters, equestrians and wheelchair users;
- Where possible, design trail corridors to accommodate more than one user group;
- Provide recreation trail amenities such as trailheads, signage, benches and litter receptacles/dog waste stations where appropriate;
- Work to construct trails pursuant to the design standards outlined within this Plan.

**Existing Conditions, Inventory and Analysis****Regional Trail System**

The Town of Foxfield shares boundaries with unincorporated Arapahoe County, City of Aurora and the City of Centennial, each contributing to an extensive network of sidewalks, bike lanes and park/open space trails within the southeastern portion of the Denver Metro area. The Cherry Creek Regional Trail is located approximately ½ mile from Foxfield's western boundary and parallels the Cherry Creek River Corridor, connecting downtown Denver to Aurora, Centennial, Parker and Franktown. Construction to complete a key missing link of the Cherry Creek Trail and provide a safe grade separated crossing under Arapahoe Road started in the winter of 2013 and is scheduled to be completed in 2015. The 9.4 mile Piney Creek Trail (about 1 mile north of Foxfield) provides an east/west aligned trail between Centennial and Aurora. Numerous other local trails and sidewalks connect neighborhoods with shopping centers, schools, parks and public facilities depicted on the Regional Trail Network Map and described further below.

**Destinations**

In addition to regional trail facilities, there are several destinations within walking distance (or in proximity to existing pedestrian or bicycle facilities) from Foxfield and the adjacent neighborhoods.

**Schools**

Schools serve as primary destinations for the youngest residents living within Foxfield and the surrounding neighborhoods. The schools that are within walking distance from Foxfield include: Our Lady of Loreto and Regis High. Most elementary students living in Foxfield attend Creekside Elementary School located

approximately one mile east of the Foxfield boundary within the Chenango neighborhood. There are no existing pedestrian facilities connecting Creekside Elementary with the residential neighborhoods surrounding the school. Foxfield's middle and high school students enrolled in the public school system attend Liberty and Grandview schools respectively. Both schools are over a mile and a half east of the Foxfield boundary.

### Retail Centers

Four shopping centers at the Arapahoe Road and Parker Road interchange serve as major destinations for many people in the area, including the Foxfield Village Center (SE Corner), Arapahoe Crossings (NE Corner), Cornerstar (SW Corner) and The Ridge at Centennial (NW Corner). These commercial areas are generally oriented toward automobile access and circulation. Pedestrians and cyclists must navigate large parking lots with few designated facilities, although it is worth noting that the newer Cornerstar development does provide pedestrian and bicycle facilities (plazas, benches, meandering sidewalks, bike racks, etc.).



Figure 1: Foxfield Village Center

### Parks, Open Space & Recreation Facilities

Trail connections to the greater regional pedestrian/bicycle system will provide residents with an alternative way of accessing the following nearby facilities: Cherry Creek Soccer Complex, Cherry Creek Valley Eco Park, Cherry Creek State Park and Reservoir, Valley Country Club, The Farm's Central Park, and the various recreational facilities including sports courts and fields that are located at many of the schools sites previously discussed.

### Local Conditions

Refer to the Existing Conditions Map for a graphic depiction of the information presented in this section.

### Existing Roads

Foxfield is bound by two arterial roads, State Highway 83 (Parker Road) to the west and Arapahoe Road to the north, which can make access to the adjacent trail networks and other destinations a challenge. Along Arapahoe Road (east of the Parker Road intersection) there are three existing signalized intersections with pedestrian crosswalks that provide access points to the existing trail network north of Foxfield. Along Parker Road (south of the Arapahoe Road intersection) there are two existing signalized intersections with pedestrian crosswalks accommodating future access to the trail network west of Foxfield. In 2012, CDOT completed the \$50 million construction project to upgrade the Parker Road/Arapahoe Road intersection. With the intent to improve traffic flow at this busy intersection, the completed project resulted in a grade-separated flyover with Parker Road elevated above Arapahoe Road.

Within the Town's boundaries there are approximately 10 miles of paved roads within a 50-60 foot public right-of-way. Generally roads are about 24 feet in width with a drainage swale on each side. Although it varies widely from one property to the next, there are instances where trees, landscaping, retaining walls, fence columns and mail box structures encroach into the public right of way and may have an impact on trail alignment and overall costs.



## Topography & Drainage

Located within the Cherry Creek drainage basin, Foxfield's rolling terrain includes a natural drainage corridor that flows west into Cherry Creek, closely paralleling Hinsdale Avenue. A second drainage corridor running through the northeastern portion of Foxfield meanders through private properties and Town open space before flowing into the new stormwater facility on the southeast corner of Arapahoe Road and Parker Road. Existing topography starts from a high point on the eastern boundary and drops approximately 200 feet at the western boundary at Parker Road. The lowest elevations occur in the southwestern side of Foxfield where the existing drainage corridor continues under Parker Road. The topography in Foxfield allows expansive views of the Rocky Mountains and southwest metro area.

## Existing Trails and Pedestrian Facilities

The existing sidewalk and trail facilities within Foxfield are located in the northern portion of the community. There is an existing sidewalk on the south side of Arapahoe Road, however, sections of this sidewalk are incomplete.

There is also an existing 10-foot wide multi-use trail which travels through the Norfolk Open Space between S. Lewiston Way and S. Norfolk Court cul-de-sac. A second segment of this trail running from Arapahoe Road (near the Parker Road Bridge) and Loop Road has been constructed, but the missing trail link across private property between the Loop Road and S. Lewiston Way has yet to be completed. Grade separated trail tunnels under both Loop Road and S. Lewiston Way were also constructed as part of the CDOT improvements to the Parker/Arapahoe Road intersection.

## Opportunities and Constraints

Through the trail planning process, several opportunities and constraints were identified with regard to safe access to regional trail networks as well as nearby schools, retail centers and other key destinations.

## Access to Existing Regional Trail Network

The existing signalized intersections along Arapahoe Road and Parker Road provide an opportunity for trail users to cross these busy arterial streets and access the surrounding trail network while oncoming traffic is stopped. The three traffic lights on Arapahoe Road located at S. Lewiston Way, Richfield Street and Waco Street accommodate access from Foxfield to Regis High School, Piney Creek Trail and the Arapahoe Crossing shopping center.

Trail users can also access the existing Cornerstar shopping center and Cherry Creek Trail (to the west) by using the existing sidewalk along the Arapahoe Road corridor. Unfortunately, the two missing segments of sidewalk along Arapahoe Road can be discouraging to residents of Foxfield and the adjacent neighborhoods that rely on this as a transportation option for access to school, work or the retail centers. The first incomplete section occurs between S. Lewiston Way (aka Ring Road) and Richfield Street. The compacted unpaved foot path along this section indicates existing pedestrian circulation on the south side of Arapahoe Road. The second incomplete section occurs east of Waco Street.

Two other existing signalized intersections on Parker Road, located at S. Chambers Way and E. Broncos Parkway, provide an opportunity for pedestrians to cross Parker Road via existing crosswalks. Without any sidewalks or trails



Figure 2: Missing Sidewalk between s. Lewiston Way and Richfield Street

along Parker Road, however, access to and from these intersections is problematic. For instance, the closest existing sidewalk to the signalized intersection and crosswalk located at Parker Road and S. Chambers Way is approximately 800 feet to the west. In addition to constraints related to the speed of motorized traffic and the width of both Parker and Arapahoe Roads, pedestrians must walk along Parker Road to reach the existing signalized intersections from the E. Fremont Avenue intersection. These conditions can pose an intimidating situation even to the most experienced trail user and will be a key issue for Foxfield, Arapahoe County, Centennial and CDOT to address in order to improve connectivity between communities on each side of Parker Road. One possible solution to this issue would be a future grade separated crossing under Parker Road in line with the existing drainage channel and multi-use trail to the west depicted in Figure 3.



Figure 3: Multi-Use Trail & Drainage Facility west of Parker Road (looking toward Parker Road)

### Local Trail Access

The local trail network within Foxfield will provide connectivity to key destinations within the community as well as access points to the adjacent neighborhoods and the regional trail system. Opportunities for trail corridors within Foxfield include roads/public right-of-ways, parks and open space.

Foxfield's local road network is primarily arranged on a modified grid pattern with most streets running in a north/south or east/west orientation. Those roads with a minimum 60' right-of-way are wide enough (in general) to accommodate trail facilities in addition to the paved road and drainage swales. Richfield Street is the primary north/south through street running from Arapahoe Road to Jamison Avenue in the Chenango neighborhood. Waco Street and Buckley Road also provide some north/south connections; however, neither of these streets continues all the way to the southern boundary of town. Easter Avenue is the primary east/west through street running from Andes Circle in the Chapparal neighborhood to Parker Road via Easter Way and Fremont Avenue. Davies Avenue and Hinsdale Avenue also provide some east/west connectivity, although again, these are not through streets and terminate within Foxfield.

Opportunities for potential non-roadway trail corridors exist within the established network of overhead utility lines, utility easement and drainage easements. While the process of acquiring trail easements and/or negotiating the use of existing easements with land owners and utility companies can be lengthy and complicated at times, dual use of utility and drainage corridors for trail access is a common occurrence in other communities and is worth further exploration within Foxfield as well.

Norfolk Open Space is currently Foxfield's only park. The existing multi-use trail running through the park currently provides trail users with access to the Foxfield Village Center businesses without having to navigate or cross motorized traffic along Lewiston Way (Ring Road). Future phases of this trail corridor will provide access to the Arapahoe Road/Parker Road intersection and the sidewalk connector along Arapahoe Road, continuing west to the Cherry Creek Trail.

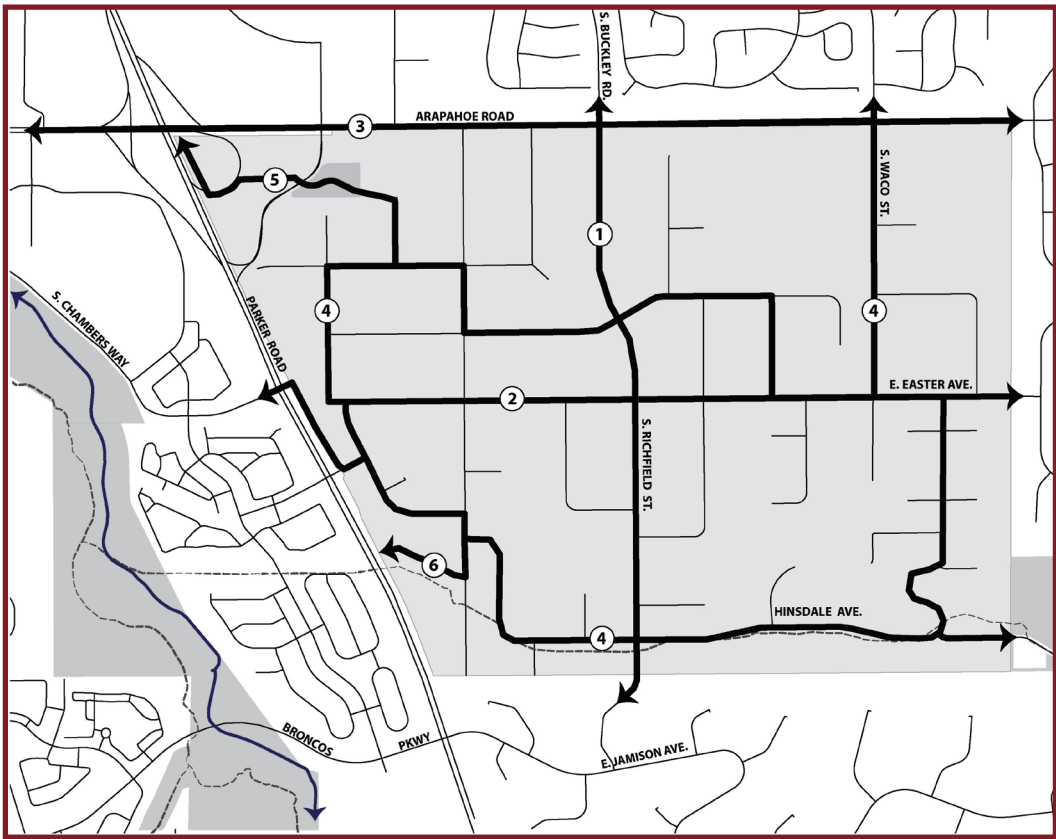


Figure 4: Norfolk Open Space

# Foxfield Trail Network

Within a regional context there is a significant interconnected trail network to the north and west of Foxfield. This Trail Plan identifies trail corridors and opportunities to further develop the regional trail network to provide trail connectivity and access to the residents of Foxfield and adjacent neighborhoods. The proposed trail routes presented below are based on input received throughout the community engagement phase of this planning process including the Trail Plan Survey, TAG planning exercise and the community workshop. Where possible, the proposed trails have been aligned to connect with existing trail facilities outside of Foxfield and to provide access to the specific destinations identified by the community. Participants at the community workshop expressed general support for the development of unpaved trails within existing fenced utility corridors, along existing road corridors and within public parks and open space land.

It is anticipated that the Foxfield Trail Network will be used by people of all ages and abilities, including area residents and possibly regional visitors. Whether utilizing the trails for recreation or to commute to and from destinations, the trails should accommodate an assorted user group including: runners/walkers, bicyclists, pet owners, in-line skaters, persons with disabilities, equestrians, wildlife viewers and nature enthusiasts. Each trail route description includes the Corridor Type and Trail Classification. Please refer to the Trail Design Guidelines section of this Plan for additional information on the corridors and classifications. The following trail routes are described in further detail below:



- 1. Richfield Trail
- 2. Easter Trail
- 3. Arapahoe Road (Sidewalk Connector)
- 4. Loop Trail
- 5. Norfolk Trail
- 6. Drainage Trail

Figure 5: Proposed Trail Network Key Map



## Primary Trail Routes

The primary routes will provide access from the Town boundaries through Foxfield in all four cardinal directions. Attendees at the community workshop showed the highest percentage of support for unpaved trails along the primary routes; however, approximately 40% expressed support for paved trails along these corridors as well. Support for either a bike route, bike lane or both along the primary routes ranged from 33% to 50% of workshop participants.

The primary trail routes will link multiple neighborhoods to key destinations within and adjacent to Foxfield. The primary trail routes are anticipated to result in higher volumes of trail use and should accommodate as many users as possible. Primary trail routes are good candidates for signed bike routes since the proposed 4 to 6-foot wide roadside trail surface is not adequate to accommodate both cyclists and pedestrians during periods of peak travel. Signed bike routes can serve to encourage more experienced cyclists to travel on the same facilities used by other vehicles, thereby limiting the number of conflicts between cyclists and pedestrians on the roadside trail. The following primary trail routes will enhance Foxfield's trail via connections to adjacent neighborhoods and nearby facilities.

### 1. Richfield Trail

**Corridor Type:** Right-of-way

**Classification:** Roadside Trail

Richfield Trail is a north/south trail connecting Foxfield to adjacent neighborhoods, schools and retail shopping centers. This trail corridor will complete an important missing trail link between the Chenango neighborhood and the Piney Creek Trail located approximately one mile north of Arapahoe Road. Access to the regional trail system north of Foxfield will be provided via an existing signalized at-grade road crossing located at the Richfield Street and Arapahoe Road intersection.

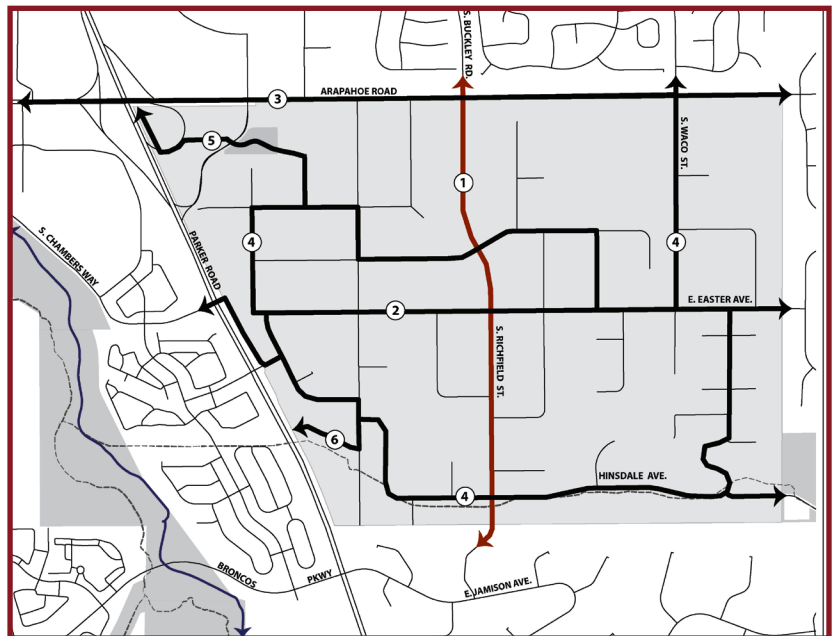


Figure 6: Richfield Trail Alignment

The placement of this roadside trail will generally be located within the existing 60-foot public right-of-way (ROW); however, there may be locations where the existing paved roadway is not exactly centered within the ROW and where additional trail easements may need to be acquired to facilitate the development of this trail corridor.

## 2. Easter Trail

**Corridor Type:** Right-of-way

**Classification:** Roadside Trail

Easter Trail is a central east/west corridor through Foxfield providing regional access between neighborhoods east of Parker Road and the Cherry Creek Regional Trail. Access to the regional trail system west of Foxfield (via the at-grade signalized crossing located at Parker Road and Chambers Way) will require close coordination and cooperation between Foxfield, Arapahoe County and CDOT to implement the trail segments outside of the Town's boundary.

Like Richfield Trail, Easter Trail will generally be located within the existing 60-foot public ROW. Sections of the trail may require an additional trail easement to maneuver around any existing utility facilities or other structures that cannot be relocated.

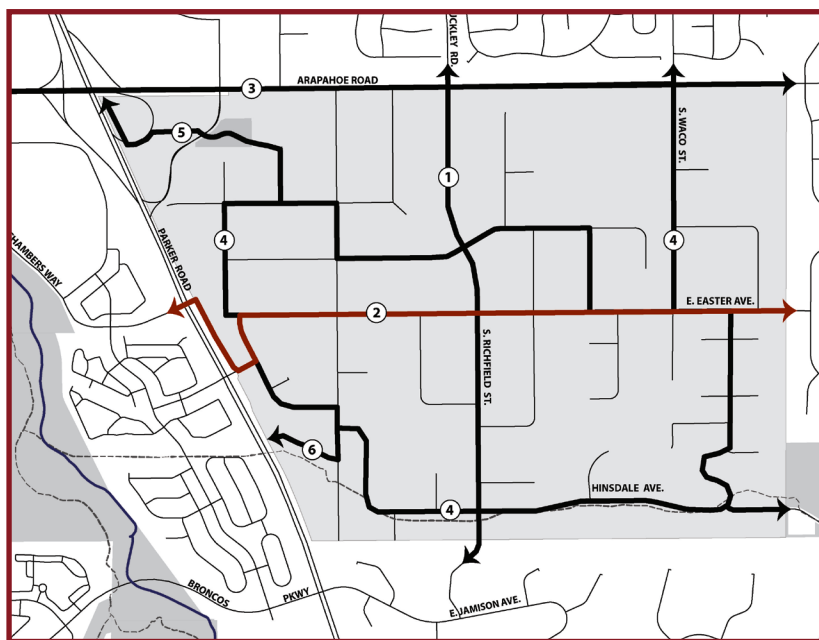


Figure 7: Easter Trail Alignment

## 3. Arapahoe Road (Sidewalk Connector)

**Corridor Type:** Right-of-way

**Classification:** Sidewalk

The existing sidewalk along the south side of Arapahoe Road is another east/west connector running along the northern boundary of the town. Although there are two 'missing links' adjacent to Foxfield, the otherwise continuous sidewalk provides one of the few points for trail users to safely cross under Parker Road to access the Cherry Creek Regional Trail. This pedestrian facility also provides access to the retail shopping centers: Foxfield Village Center and Cornerstar, as well as off-site sidewalk facilities that connect to Regis High School.

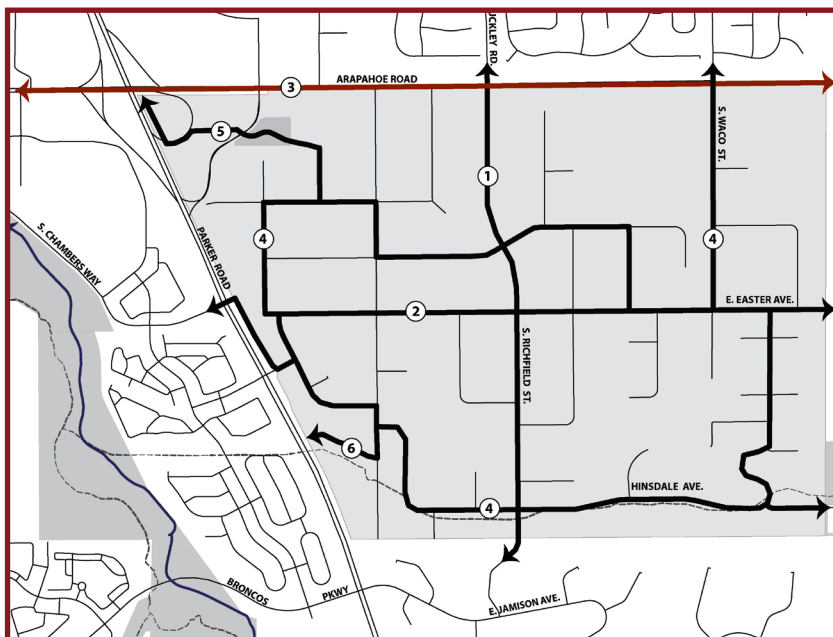


Figure 8: Arapahoe Road Sidewalk Alignment

## Secondary Trail Routes

Using the analogy that the primary trail routes serve to connect several neighborhoods with key destinations, in the same manner that an arterial or collector street provides access between cities and towns, a secondary trail route may be seen as serving a similar purpose as local streets. Secondary routes provide trail connectivity between Foxfield's individual properties as well as providing options or alternatives when accessing primary routes or key destinations.

### 4. Loop Trail

**Corridor Type:** Right-of-way

**Classification:** Roadside Trail

Loop Trail will link up with all three primary trail routes and will provide trail access to almost half of the residential properties within Foxfield.

When presented with the option of a paved or unpaved surface for this corridor, community workshop attendees indicated much stronger support for an unpaved trail (78%) than a paved trail (28%). Although an unpaved trail surface may deter some 'wheeled' user groups like cyclists and in-line skaters from using this trail, an unpaved surface treatment is sometimes more popular with runners and walkers. An unpaved surface for the Loop Trail would also be appropriate for equestrians as long as the minimum height clearance can be maintained. Loop Trail can be generally located within the existing 60-foot public ROW. However, portions of the trail may require an additional trail easement to maneuver around existing facilities that cannot be relocated.

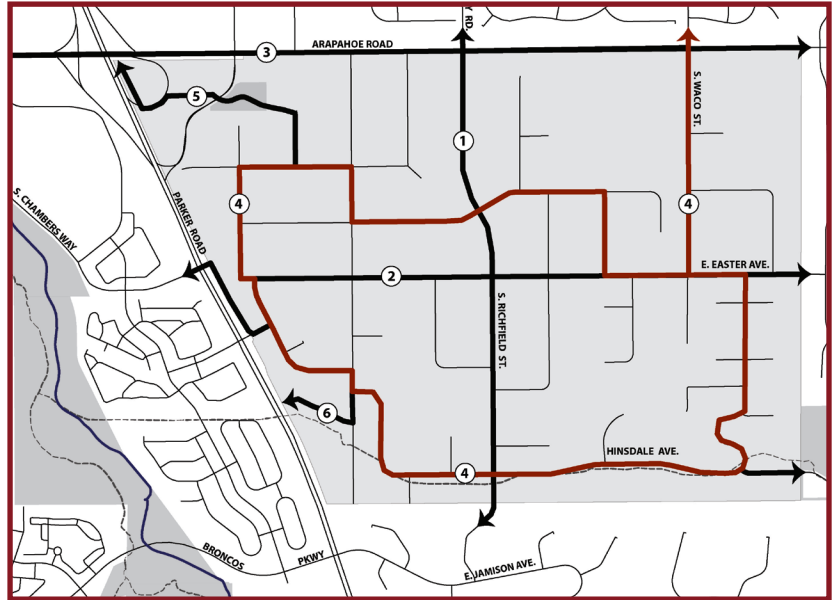


Figure 9: Loop Trail Alignment

### 5. Norfolk Trail

**Corridor Type:** Greenway

**Classification:** Paved Multi-Use

Norfolk Trail is an existing multi-use trail that runs in an east/west direction and connects from Norfolk Court to the Foxfield Village Center (FVC) through the Norfolk Open Space. The second phase of this trail will continue northwest (behind FVC) to the signalized intersection at Parker and Arapahoe Road, providing access to the Arapahoe Road sidewalk which continues west and connects to the Cherry Creek Regional Trail. This trail corridor includes two existing trail underpasses and upon completion of the missing trail links, users will be able

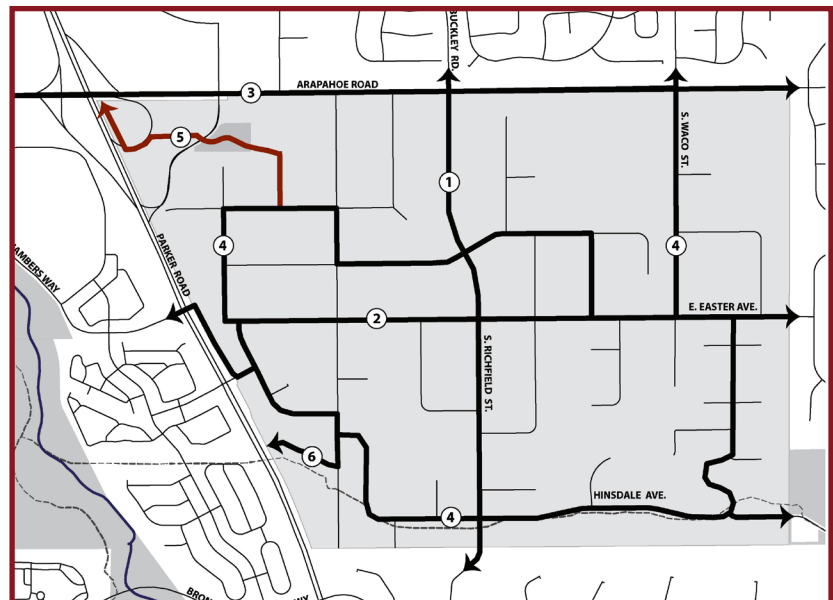


Figure 10: Norfolk Trail Alignment

cross both Ring Road and Loop Road without having to navigate vehicular traffic. The trail's transition in elevation from the low-lying open space parcel up to the businesses at FVC is achieved from an existing ramp that winds its way up to a sidewalk on the west side of Loop Road. Figure 11 shows a worn footpath up the hill indicating that some pedestrians prefer to take a more direct route from the trail underpass up to the Village Center parking lot. Stairs leading up to the parking lot could be constructed here to provide a second, more direct pedestrian access.

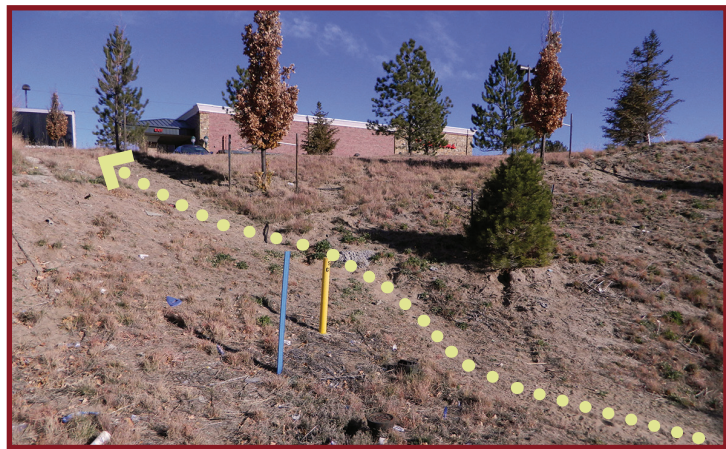


Figure 11: Proposed Location of Future Stairs

## 6. Drainage Trail

**Corridor Type:** Greenway

**Classification:** Paved Multi-Use

Appropriately named, the Drainage Trail would parallel the existing natural drainage channel between Buckley Road and Parker Road across the currently undeveloped parcel. This trail facility could be accommodated by either a trail easement or dedicated open space. If additional open space is acquired by the Town, provisions should include a trailhead with parking and relevant amenities like signage, benches and trash receptacles at a minimum.

This trail corridor has the potential to benefit regional trail users by providing a safe and convenient way to cross under Parker Road (via the proposed off-site trail tunnel) and continue west along the existing multi-use trail west of Foxfield. This would provide residents of Foxfield, Chapparral and Chenango with an alternative point of access to the Cherry Creek Regional Trail. To do this, the Town of Foxfield will need to partner with Arapahoe County, the City of Centennial and CDOT to further investigate the feasibility of this regional trail connection.

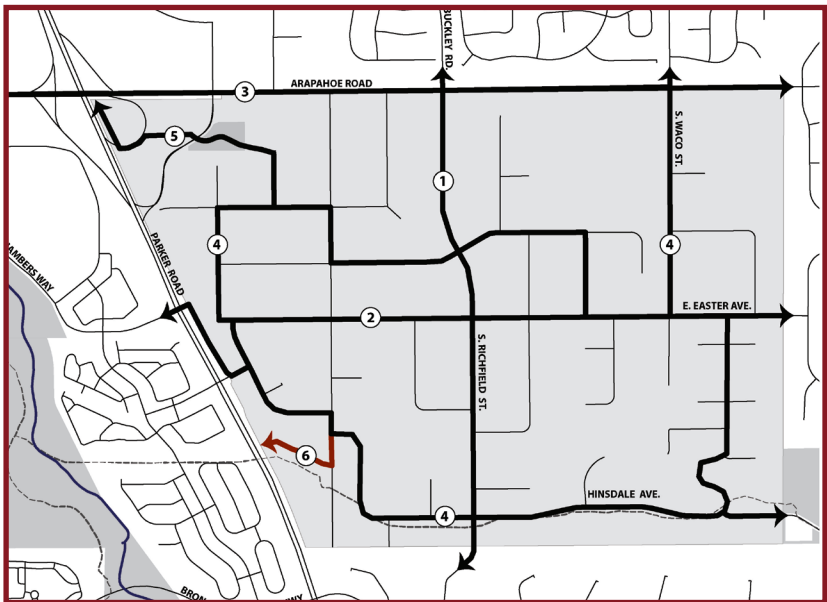


Figure 12: Drainage Trail Alignment



# Design Guidelines

Trails within Foxfield should be constructed to address the specific needs of the various groups that will be utilizing the local and regional trail network, whether for recreation purposes or as a means to get from one destination to another. Trail user groups in Foxfield will include (but are not necessarily limited to) pedestrians, cyclists, and equestrians. The following guidelines will ensure that trail facilities developed by both public and private entities will be constructed to uniform standards and will accommodate a diverse user group.

## Types of Trail Corridors

There are several types of corridors that can potentially serve trail development within Foxfield. These include existing road right-of-ways, drainage corridors, utility or drainage easements and easements that cross private property. Development and planning for trails in each corridor type should consider the unique conditions and environments each type presents. The following is a breakdown of the various types of trail corridors that may be utilized for the development of trails in the community.

### Right-of-way Trail Corridor

Public right-of-ways (ROW) are linear corridors that provide a means to travel between destinations. Facilities within the public ROW (like roads, sidewalks, bicycle lanes/parking, bus stops, etc.) determine the transportation methods used to get from place to place. Pedestrian facilities within the public ROW vary drastically from one community to the next. In Foxfield, it is anticipated that these types of trails will be unpaved. Developing pedestrian facilities with the existing ROW corridors will transform the public ROWs and provide residents with options for how they travel.

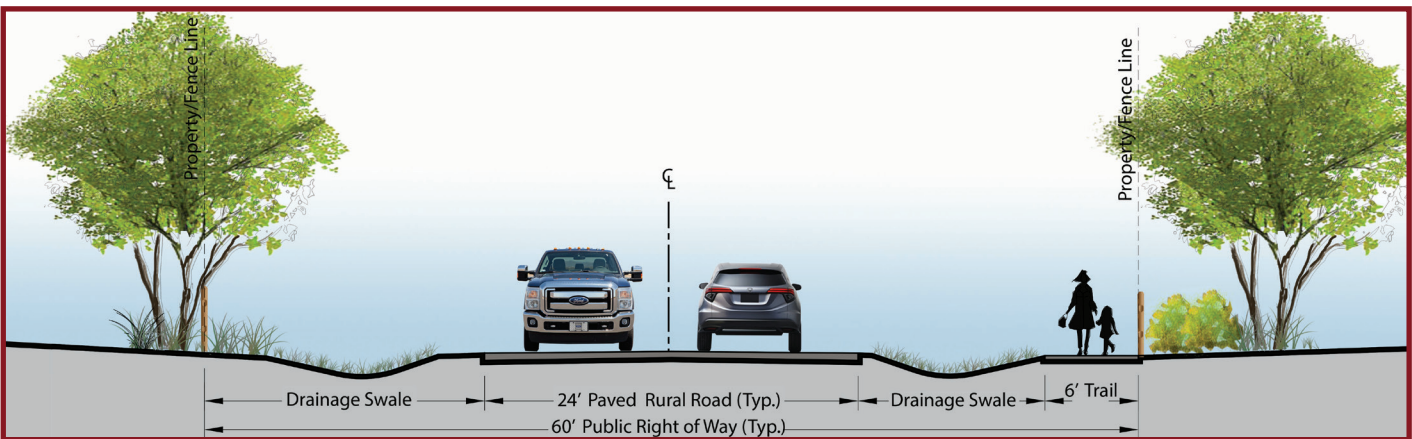


Figure 13: Typical Right-of-Way Corridor Cross Section

## Open Space Corridors

### Drainage Channel

Drainage channels and riparian corridors function to improve water quality by filtering stormwater runoff and pollutants before returning to the rivers and reservoirs that provide drinking water. Trails constructed adjacent to drainage ways and floodplains need to preserve a buffer zone to protect the existing vegetation and wildlife that rely on riparian habitats to survive. Trails within drainage corridors provide users an opportunity to view and interact with active natural systems and experience the contrasts as the seasons transform. Interpretive signage along these corridors can be used to educate people about the history, ecology or significance of the place. Drainage channel trails are generally paved and wide enough to accommodate service vehicles that may need access to the drainage channel for maintenance. If maintenance access to the drainage channel is not necessary, or a permeable surface is more desirable, then a soft surface trail may be considered.



## Greenway Trail Corridor

Greenway trail corridors are those located within parks and dedicated open space land. These areas preserve undeveloped land and provide a destination to the general public for outdoor recreation, socialization and reflection. Trails within public parks generally provide access to park facilities (picnic sites, parking, fields and sports courts, etc.). Greenway trails can also be found along linear open space corridors that travel longer distances and serve to link two or more places. It is not uncommon to find a greenway trail corridor that parallels another linear facility like a watercourse, freeway, railroad or similar. Like Drainage Corridor Trails, greenway trails are paved and wide enough to accommodate two or more user groups, unless they are located within a sensitive habitat or poorly drained areas where boardwalks or natural surface trails may be more appropriate.

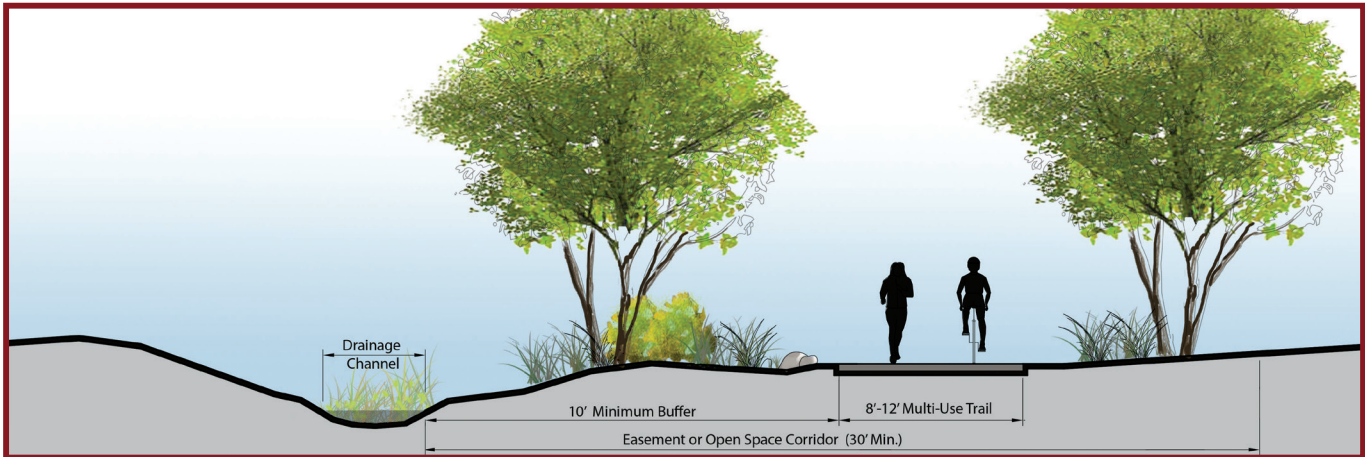


Figure 14: Typical Greenway or Drainage Channel Corridor Cross Section

## Easement Corridors

Trail easements provide access across private property outside of the public right-of-way or dedicated open space. Like greenway corridors, trail easements provide opportunities for trail use separate from the motorized vehicle transportation network, providing a different user experience than trail corridors adjacent to roads. Trail easements can accommodate almost any trail surface material.

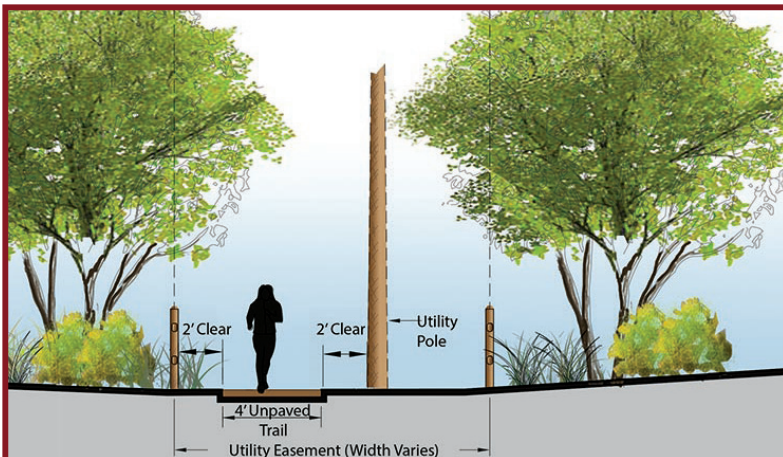


Figure 15: Typical Easement Corridor Cross Section

## Trail Classifications

The type of trail that is constructed within any of the above corridors ultimately depends on the various user groups that the trail will accommodate. Paved trails are appropriate for pedestrians, cyclists, rollerbladers, people using wheelchairs and in some cases even battery-operated vehicles such as scooters and golf carts. Runners and equestrians may feel more comfortable traveling on an unpaved trail surface. A well-planned trail network will provide a diverse system of trails that can accommodate as many user groups as possible.

### Paved Multi-Use Trail

Paved multi-use trails, typically composed of asphalt or concrete, are usually designed to accommodate the weight of maintenance or emergency vehicles that may need to access the trail from time to time. Concrete trails tend to be more durable, but the upfront cost to construct the trail is greater. Asphalt trails can be cheaper to construct (depending on oil prices), but generally have more maintenance costs with regard to resurfacing and repair during the life of the trail.



Figure 16: Paved Multi-Use Trail

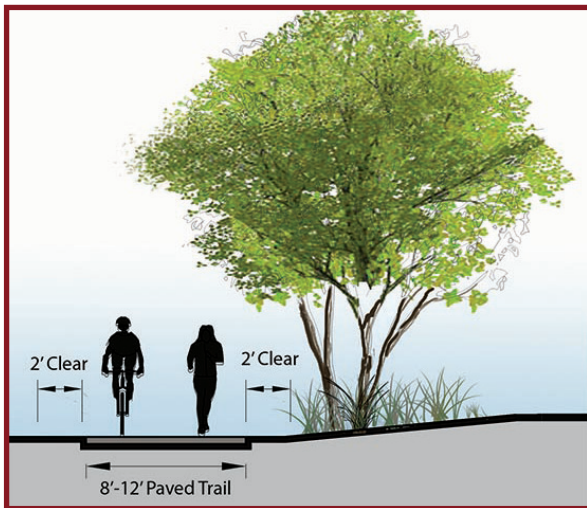


Figure 17: Typical Multi-Use Trail Cross Section

**Trail Width:** 8' minimum, 12' preferred

**Trail Surface:** Paved

**Minimum Clearing:** 10' minimum vertical clearance, 2' minimum horizontal clearance (both sides)

**User Groups:** Walkers, runners, cyclists, strollers, skateboarders, in-line skaters and disabled users including those needing wheelchairs, walkers and electric powered scooters.



## Dual Tread Multi-Use Trail

Dual tread multi-use trails generally meet the needs of all trail user groups because they include both a paved and unpaved split trail and should be considered where anticipated usage is high or user conflict is a concern. The soft surface trail is often attached to the paved portion; in other instances, the dual surface trail is separated by a vegetative buffer, which minimizes conflicts between various user groups.

**Trail Width:** 6'-8' (paved tread), and 2'-4' (unpaved tread)

**Trail Surface:** Split Paved/Unpaved

**Minimum Clearing:** 10' minimum vertical clearance, 2' minimum horizontal clearance (both sides)

**User Groups:** Equestrians and all user groups listed under paved multi-use trail.



Figure 18: Split Dual Tread Multi-Use Trail

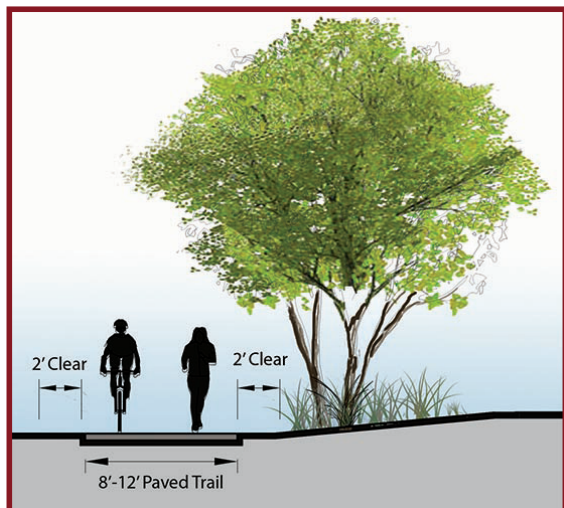


Figure 19: Typical Dual Tread Multi-Use Trail Cross Section

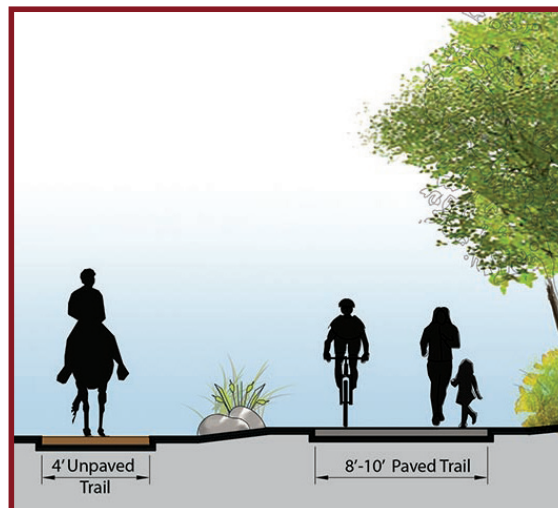


Figure 20: Typical Split Dual Tread Multi-Use Trail Cross Section

## Footpath/Equestrian Trail

Unpaved footpaths and equestrian trails are appropriate where trails are anticipated to be used less frequently or in locations where a natural surface trail is more suitable for the environment or the desired user group. These types of trails can cost less than a paved trail to install, but they generally have higher maintenance costs because they tend to deteriorate quickly and are not as appropriate in corridors that flood on a regular basis. Recommended surface materials include compacted soil, road base, graded aggregate stone or crusher fines (decomposed rock).



Figure 21: Unpaved Trail Section

Wood or steel edging may be necessary to keep surface materials in place and grasses or other vegetation out. Shredded wood fiber, chipped wood, bark mulch and other similar materials are not recommended for a trail surface as it can be easily displaced with wind and heavy use and may need to be replaced on an annual basis.

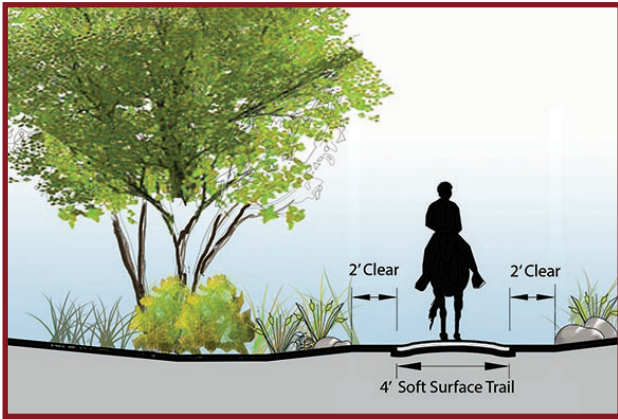


Figure 22: Typical Footpath/Equestrian Trail Cross Section

**Trail Width:** 4' minimum

**Trail Surface:** Unpaved/soft surface

**Minimum Clearing:** 9' minimum vertical clearance (12' for equestrian use), 2' horizontal clearance (preferred)

**User Groups:** Non-wheeled user groups including: walkers, runners, and equestrians.

## Sidewalk & Roadside Trails

Sidewalks and roadside trails are primarily used by pedestrians; however, depending on the level of experience and the trail surface material, they may also be used by several other user groups. These facilities may be 'attached' and located directly adjacent to the paved road/curb and gutter or they may be 'detached' and separated from the road by a landscape strip, row of trees or drainage swale. Sidewalks and roadside trails are generally located within the public right-of-way. In some situations, they can also be located outside of the right-of-way on private property within a trail easement.

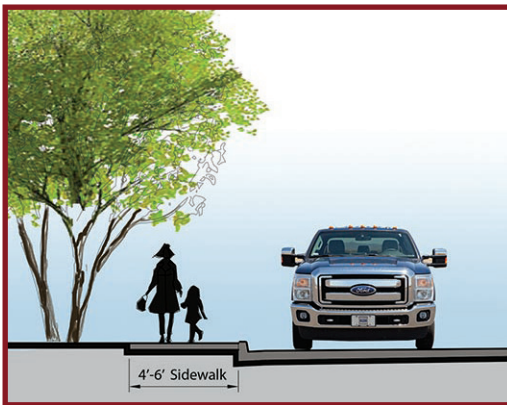


Figure 23: Typical Sidewalk Cross Section

**Trail Width:** 4'-6'

**Trail Surface:** Sidewalk: paved; Roadside Trail: paved or unpaved

**Minimum Clearing:** 10' minimum vertical clearance, 2' minimum horizontal clearance (preferred)

**User Groups (unpaved):** Walkers, runners, and younger/inexperienced cyclists. Equestrians may also use unpaved roadside trails; however, where possible, separate equestrian facilities should be considered to minimize conflicts with vehicles and bicycles.

**User Groups (paved):** Skateboarders, in-line skaters and, disabled users including those users needing wheelchairs, walkers and electric powered scooters, and all users that would also use an unpaved trail.

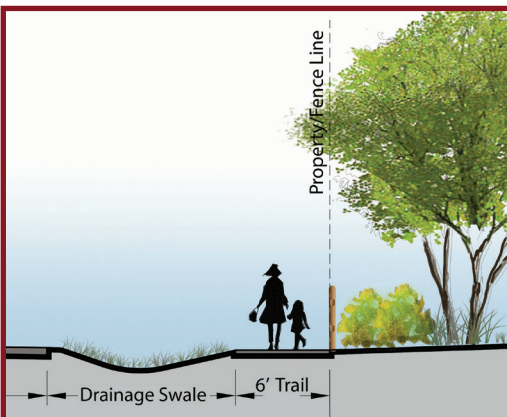


Figure 24: Typical Roadside Trail Cross Section



Figure 25: Paved Roadside Trail



## Bicycle Facilities

A common misconception exists that the public streets are only for cars, motorcycles, trucks and other road certified vehicles powered by an engine. However, in Colorado bicyclists have just as much right to travel on public roads as those driving a car, with the exception of rare situations where cyclists are specifically prohibited. With that said, cyclists are also required to obey the same laws that apply to motorists when traveling on public roads.

Essentially, almost all public roads, highways and streets already serve as bicycle facilities. It is ultimately the existing conditions (i.e., road width, road shoulders, volume and speed of traffic, frequency of large/heavy vehicles and pavement surface condition) and experience that determine the frequency of bicycle use on any given road. Local residential roads upon which fewer daily trips are made primarily by personal vehicles may not have the volume or traffic speeds to necessitate a marked or signed bicycle facility. Busier roads that include work service and delivery vehicles or large trailer trucks that travel at higher speeds generally warrant a defined bicycle facility such as a dedicated bike lane or signed bike route.

### Dedicated Bike Lane

Bike lanes tend to be used by the more experienced cyclist, comfortable enough with using the same road surface that is primarily dominated by cars, trucks, buses, etc. Bike lanes are an on-road facility and should not be separated from other motor vehicle lanes. They offer a separate lane specifically for cyclists traveling in the same directional flow as vehicle traffic and help to minimize conflicts that can occur when cyclists share a trail facility with other user groups like pedestrians.

A dedicated bike lane includes a painted (striped) lane typically located on the right side of the road. When a bike lane is present, traffic laws require that vehicles yield to cyclists when crossing over or making a right turn. Bicycle traffic within bike lanes should always travel in the same direction as the travel lane that it is adjacent to and lanes should always be provided on both sides of any two-way road. In Foxfield, the typical 24-foot wide paved road is comprised of a single 12-foot lane for each direction of travel. While travel lanes can be narrowed down to a 10-foot width in each direction, it is not likely that Foxfield will be able to implement bike lanes without widening the pavement to at least 28 feet. Bike lane standards have been provided below for reference if future roads in Foxfield are improved to accommodate this type of facility.

**Lane Width:** 5' minimum when adjacent to a street curb or on-street parking lane (can be reduced to 4' on roads with a shoulder instead of curb and gutter)

**Trail Surface:** Paved roadway

**Minimum Clearing:** Consistent with standard road clearances

**User Groups:** Advanced cyclists



Figure 26: Dedicated Bike Lane (no curb)

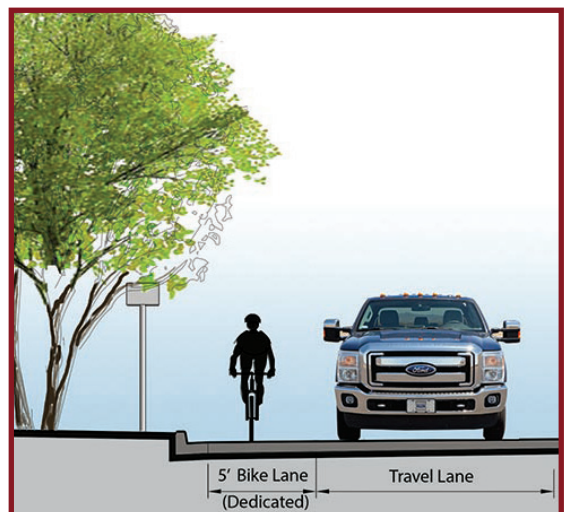


Figure 27: Typical Bike Lane Cross Section

## On Street Bike Route

Bike routes generally occur either as a paved road shoulder or as a wide outside traffic lane that is shared by both drivers and cyclists. Bike routes usually have signage adjacent to the road and, in some cases, will also include a 'sharrow' or similar pavement marking to help provide directional cues to the cyclist while alerting all road users of the shared space. Among all the bicycle and trail facilities presented, bike routes are likely the least expensive to implement and may be appropriate within the Richfield or Easter trail corridors.



Figure 28: Signed Bike Route



Figure 29: Typical Bike Route (shared road)  
Cross Section

**Lane Width:** 2' when adjacent to a 12' travel lane, 4' when adjacent to a travel lane less than 12'

**Trail Surface:** Paved (asphalt or concrete)

**Minimum Clearing:** Consistent with standard road clearances

**User Groups:** Advanced cyclists

## Trail Surfacing

### Crushed rock

Sometimes referred to as crusher fines, breeze, decomposed granite or Trail Surface Aggregate (TSA), crushed rock is a very effective unpaved trail surface especially for walkers, runners, equestrians, off-road cyclists and sometimes even users of wheelchairs when trail grades (slopes) have been designed accordingly. The downside of this surface material is that trails constructed of crusher fines can result in rutting from all users when used under slightly wet or saturated conditions.



Figure 30: Crushed Rock Material and Trail



## Road Base

Road base is a mixture of clay and particle/rock 3/4 inch in diameter or larger. It can be more resistant to deformation by user traffic than trails constructed of crushed rock. It is primarily used in rural areas where unpaved roads or driveways are common and can be effective for unpaved trails used by walkers, runners, equestrians, off-road cyclists or where routine maintenance vehicle access is desired along the trail corridor. Road base is also used as a stabilizing base material for paved surface trails of either concrete or asphalt.

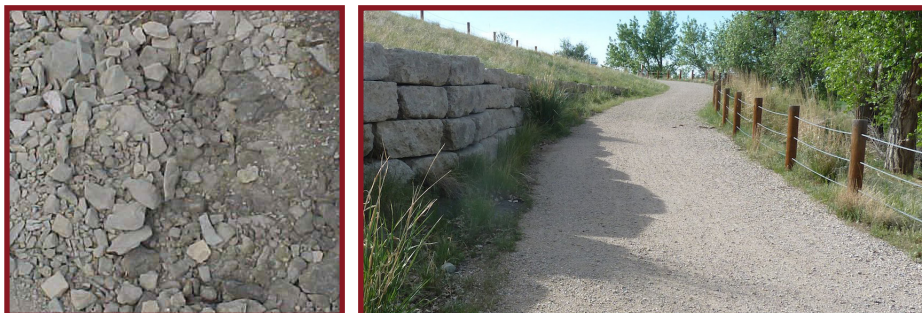


Figure 31: Road Base Material and Trail

## Asphalt

Hot mix asphalt, sometimes called asphalt concrete pavement, can be cost effective where paved trails are desired. Asphalt trails should include an 'edging' treatment for containment, stability and long term viability. Asphalt surfaces can be softer than concrete and may be more desirable for walkers or runners. When maintained in good condition and patched as needed, asphalt is also an excellent material for cyclists, in-line skaters and disabled users.



Figure 32: Asphalt Material and Trail

## Concrete

Concrete surface trails are initially more expensive than the other materials listed above (in general), but it is the most durable material and requires the least amount of maintenance over the life of the trail. It is a less desirable surface for joggers and generally not suitable for equestrians. However, it can be a very desirable surface for people pushing strollers, in-line skaters, skateboarders, cyclists and disabled users. Concrete trails should include a non-skid medium broom finish for trail safety and accessibility.



Figure 33: Concrete Material and Trail

## Recycled Paving Material

Recycled paving materials including recycled hot-mix asphalt concrete and rubberized asphalt concrete incorporate small pieces of old tires or reclaimed asphalt paving with a percentage of virgin asphalt concrete paving. Generally recycled paving is installed over a concrete or asphalt base, so while it may be an environmentally friendly option, it may not be as cost effective as some of the other trail surface materials mentioned above. Proper drainage is also crucial for the longevity of the trail surface.



Figure 34: Recycled Paving Material and Trail

## Trail Road Crossings

### At-Grade Crossing

Any time a trail crosses a street there exists a potential safety hazard. An at-grade trail crossing, which is at the same level as the roadway, requires both the motorist and the trail user to be aware of the potential conflicts and the risk of collisions. Methods of traffic control such as signage, painted crosswalks, flashing lights, full stop intersections, reduced speed limits and high-profile crossings (or humps/speed bumps) should be considered at locations where trail users will need to cross roadways. Crossing locations should be logical, clearly visible and relatively flat in order to maintain an effective sight line.



Figure 35: Roadway/Trail Crossing

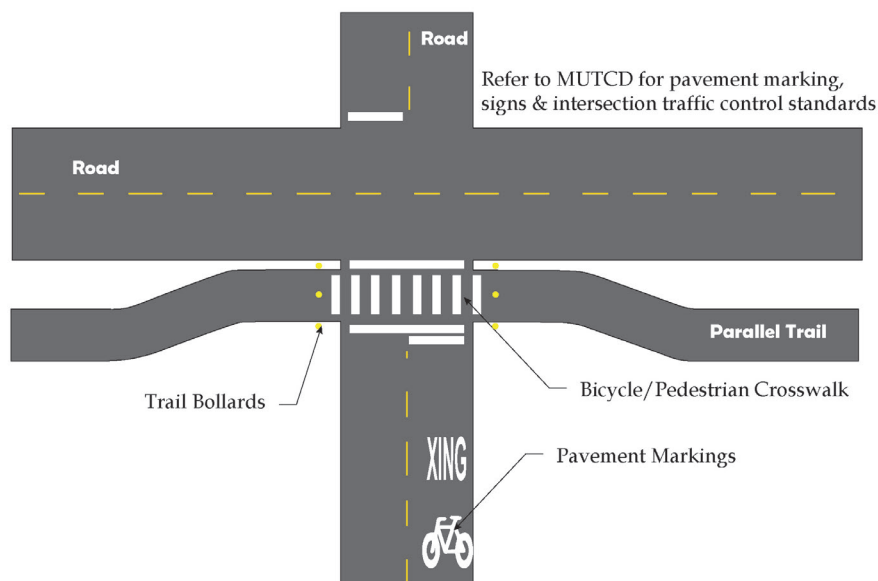


Figure 36: Typical Trail Crossing at Road Intersection



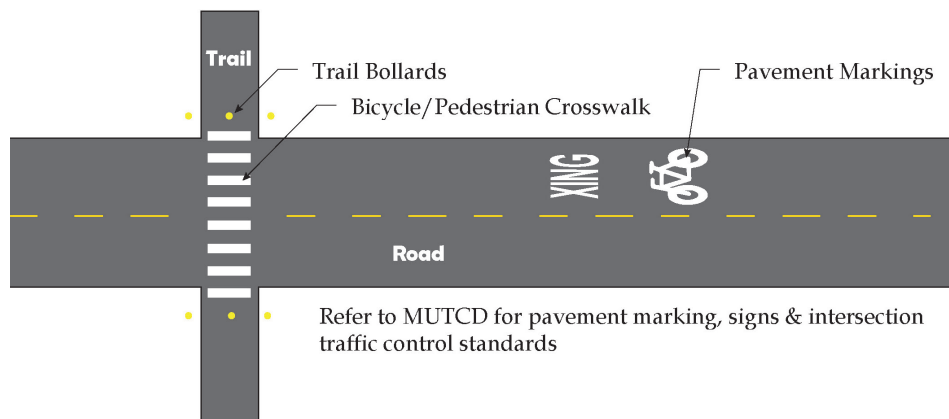


Figure 37: Typical Trail Crossing (mid block)

### Trail Underpass (grade separated crossing)

A trail underpass can be used to avoid requiring trail users to navigate crossing roads with high volumes of traffic or higher speed limits. Underpasses typically utilize an existing roadway bridge or a drainage culvert large enough to accommodate trail users. Vertical clearances of underpasses should be a minimum of 10 feet in height (12 feet if used by equestrians) with a minimum width of 12 feet. The underpass must be designed to allow for proper drainage, and lighting will increase safety. Trail underpasses installed as part of a larger roadway improvement project may be the most cost effective; however, this type of facility will still include a hefty price tag.



Figure 38: Existing Trail Underpass at S. Lewiston Way (Ring Road)

## Trail Facilities

### Trailheads

Trailheads are critical to the trail network system. The trailhead serves as a link between various transportation systems and the trail network while establishing access points that are accessible to everyone. Trailheads should be highly visible and should take into consideration the diversity of trail users and the overall function of the specific trail. Trailheads also provide the necessary useful information that shows users where they are along certain trails, where specific trails lead and more importantly, how to get there.

Trailheads can be separated into two categories: primary and secondary. Primary trailheads generally provide a wide range of amenities including parking, signage, picnic facilities, benches, trash receptacles and sometimes even restroom facilities. Primary trailheads are usually located at key destination points, such as a park, school or other public facility.

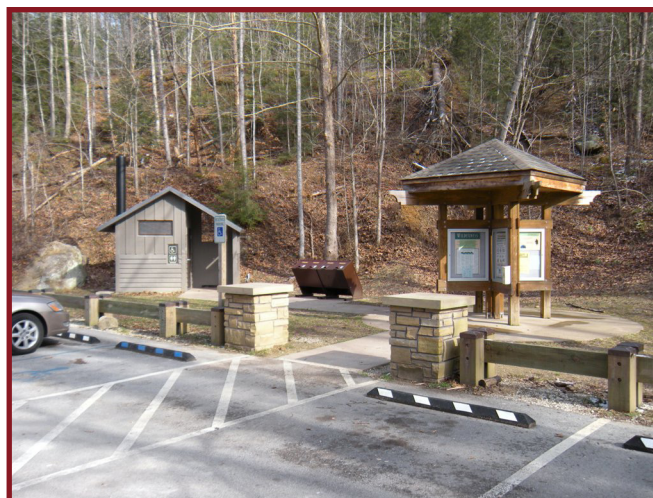


Figure 39: Primary Trail Head with Parking

Secondary trailheads are less elaborate and may offer only a few amenities or no amenities at all. Secondary trailheads are generally located near a major roadway intersection or periodically along a fenced trail corridor for access. During the Trail Plan Community Workshop, participants generally preferred images of minimal trailheads that were not very elaborate.



Figure 40: Secondary Trailhead

## Signage

Adequate signage is essential for trails to alert users to potential conflicts and to convey regulatory messages. Signs that help to guide trails users will indicate direction, destinations, distance and cross street names.

- **Trailhead Signage** provides a reference for the location where a user may access the trail system and usually will include a map of the trail corridors, any specific rules or regulations for trail use and the distance to trail intersections or key destinations from the trailhead location.



Figure 41: Trailhead Signage

- **Directional/Informational Signage** helps facilitate the user's comfort and confidence. It is generally constructed to a pedestrian scale and should be no more than 5 or 6 feet in height. Directional signage should be provided at trail intersections and may also include a list of specific destinations that will be reached by traveling along the corridor. Maps of the trail networks should be placed at trailheads. Cardinal directions should be noted on directional signage or distance markers to assist with orientation.

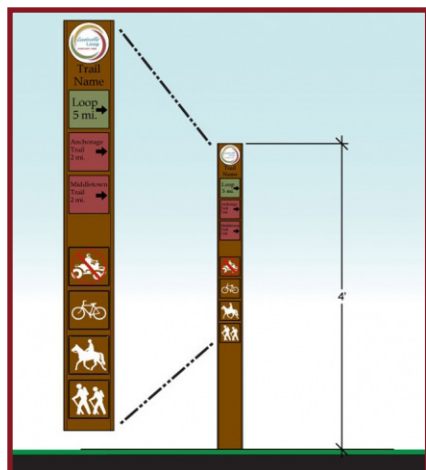


Figure 42: Distance Marker and Directional Signage



- **Educational or Interpretive Signage** provides the trail user with information about a specific site or trail corridor. Generally they will include graphics and descriptive text that is intended to interest and educate the user about social, environmental or ecological topics including existing flora, fauna, geologic formation or local history/heritage along the trail corridor. Interpretive signs should be constructed of a high quality and durable material. Typical signage sizes include 15" × 18" or 24" × 36".



Figure 43: Interpretive Trail Signs

- **Regulatory and Warning Signs** should generally conform to the Manual on Uniform Traffic Control Devices (MUTCD) standards. Regulatory signs inform users of traffic laws or regulations and indicate legal requirements that are not readily apparent. Warning signs give motorists and trail users advance notice of unexpected conditions on or adjacent to a road or trail, and of situations that might not be readily apparent.



Figure 44: Typical Regulatory and Warning Sign

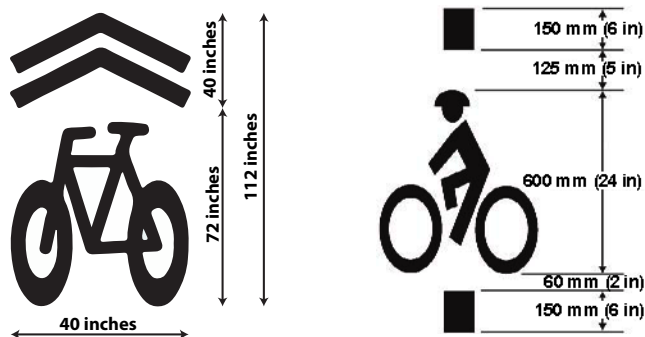


Figure 45: Sharrow Pavement Marker and Bike Lane Pavement Marker

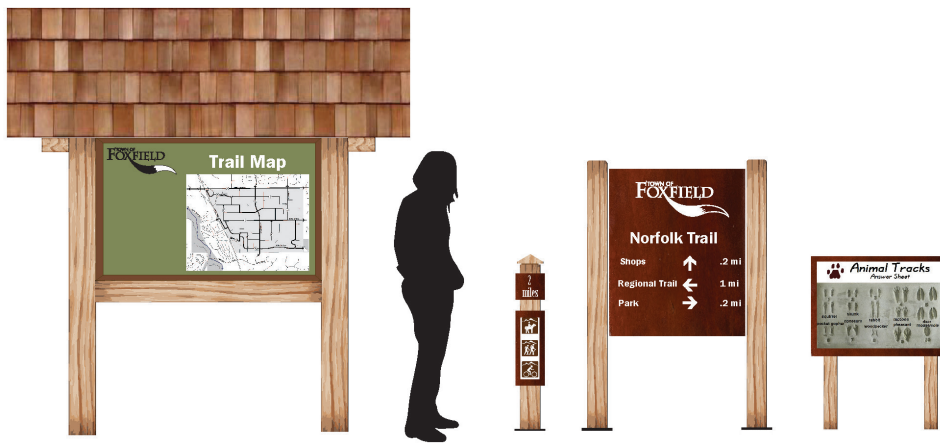


Figure 46: Example of a Cohesive Trail Sign Family (Trailhead, Distance Marker, Directional and Educational Signs)

## Trail Furnishings

### Bike Racks

Bike racks provide parking for cyclists at a given destination including retail centers, churches, parks, public facilities, bus stops, etc. Bike racks should be placed in a location where they are clearly visible from the approaching sidewalk or trail and generally within 50 feet of a building entrance. Bike racks need to maintain a minimum 2-foot distance from a parallel wall or 3 feet from a perpendicular wall. Inverted 'U' style racks are recommended because they support bikes in two locations and can be spaced to accommodate a bike on each side of the rack. Maintain a minimum of 30 inches on center separation between each inverted U rack.



Figure 47: Inverted 'U' Bike Rack



Figure 48: Trail Bollards

### Bollards

Bollards are used to prevent vehicles from entering trail corridors and to provide a separation barrier between vehicles and trail users. They come in a multitude of shapes and sizes and can even be removable to allow maintenance or emergency access to trail corridors if needed.

### Fencing

Fencing helps to define a trail corridor and provide separation between public and private property. Fencing may also be needed for safety purposes in some locations. Locations where fencing may be installed include along elevated pathways, steep side slopes or adjacent to busy highways or roads. In rural areas, fencing also helps to protect livestock by preventing potential conflicts with trail users. Fencing styles and materials are limitless, but should reflect the character of the surrounding area.

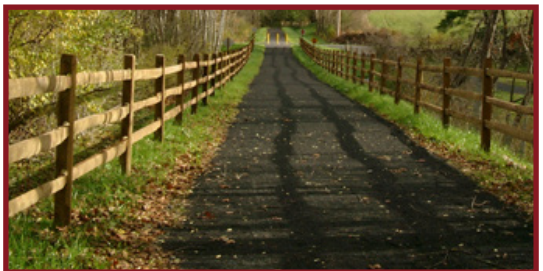


Figure 49: Trail Fencing

### Benches

Benches are generally located at trailheads and at intervals along the trail corridor or at key trail intersections. Benches adjacent to a trail should be set back a minimum of 3 feet from the trail edge and secured to a concrete pad.



Figure 50: Trail Bench



## Trash Receptacles

Trash receptacles are recommended along trail corridors, specifically those that have regular daily use. The placement of trash receptacles needs to be convenient to both trail users and maintenance personnel; it is common to see trash cans located near benches as that is where trail users tend to congregate. It is important to keep in mind that a regular trash collection schedule will need to be implemented to minimize conflicts with wildlife and maintain clean facilities.

## Dog Waste Station



Figure 51: Wildlife Proof Trash Receptacle



Figure 52: Trash & Recycling Receptacles with Bench

The accumulation of pet waste in an area can pose a potential health risk to humans, be detrimental to sensitive environments and seep into water systems. Dog waste can become concentrated along trail corridors since many people will bring Fido along. Waste stations provide small plastic disposable bags and are generally located at primary trail access points or intermediate spots along the trail. They are convenient for trail users and offer an opportunity to remind users to pick up dog waste.



Figure 53: Dog Waste Stations

## Recommendations for Trail Amenities

Consideration of trail amenities should occur at the time a specific trail route is developed. The following are recommendations with regard to placement of specific types of trail amenities.

- Benches and trash receptacles should be placed at trailheads and at waiting/resting areas.
- Bike racks should be placed at locations where cyclists are likely to dismount.
- Bollards should have reflective surfaces, be removable and be placed where motor vehicles have potential access to trails.
- Distance markers should be placed at the beginning of major trailheads and at locations where there is high recreational use. The markers should be placed at ½ mile to 1 mile intervals otherwise.
- Guard rails/fences should be a minimum height of 42" and used where there is more than 30" vertical drop-off at edge of the trail shoulder or in areas where a trail is adjacent to private property.

## Trail Maintenance

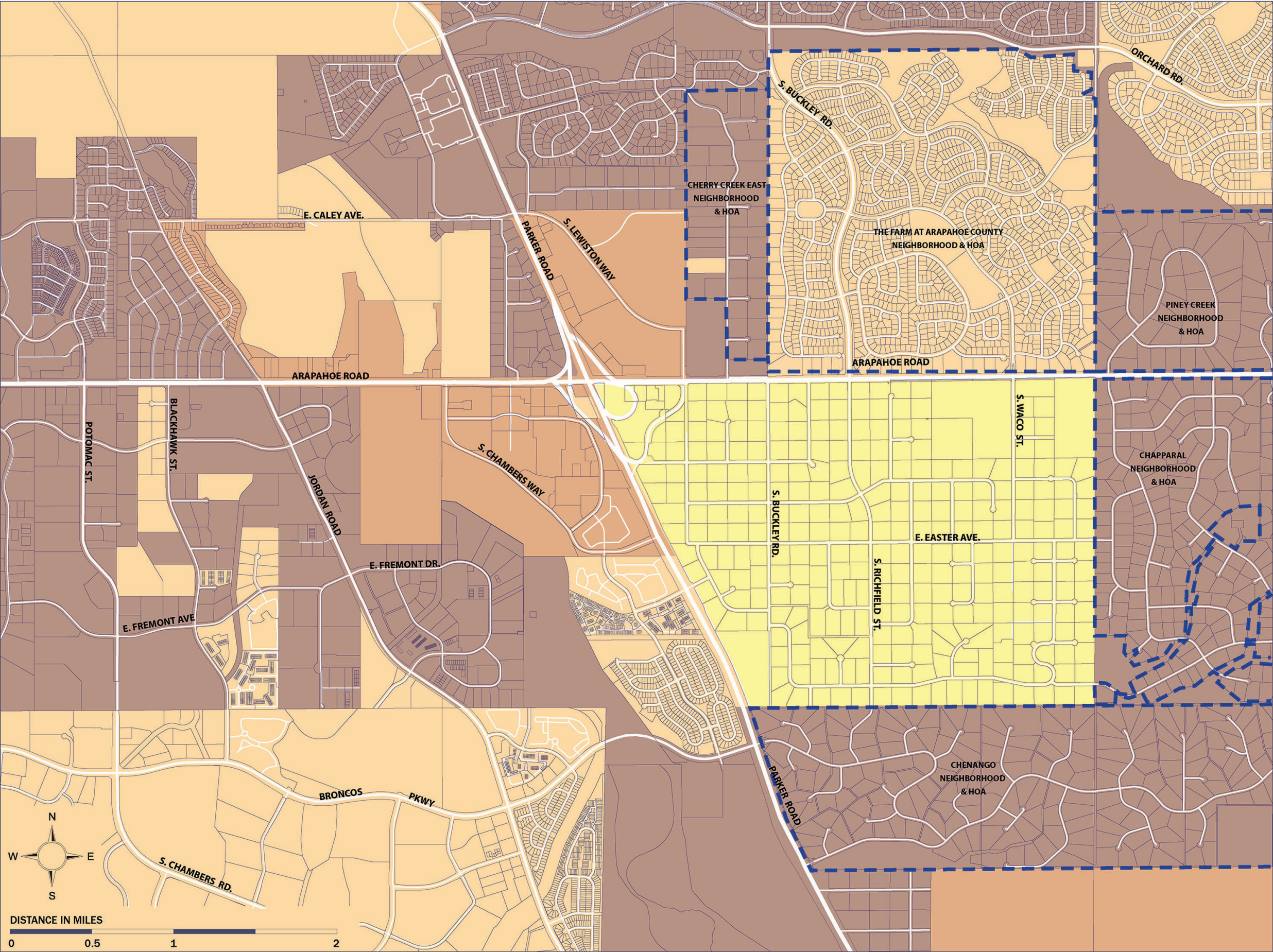
Maintenance of the trail network will be necessary for the sustainability and longevity of the trail. Prior to the construction of any trail segment, careful consideration should be given to the financial responsibility of maintaining the trail network. Selected trail furnishings and amenities should be consistent throughout the network to accommodate efficiency in repairs. They should also be durable enough to withstand generations of public use and exposure to the elements. Seasonal and weekly maintenance including trailside mowing, snow removal, trash collection and surface repairs will require equipment and valuable man hours. The following is a basic list and timeframe for various trail maintenance tasks. The Town should consider hiring personnel and/or establishing a committee of volunteers to conduct regular trail maintenance. Refer to the Trail Phasing & Implementation Matrix in the following section for additional information on trail maintenance.

Frequency	Trail Maintenance Tasks	Responsible Party
As Needed	<ul style="list-style-type: none"><li>■ Tree/brush clearing and mowing</li><li>■ Sign replacement</li><li>■ Map/signage updates</li><li>■ Trash removal/graffiti clean-up</li><li>■ Replace/repair trail amenities/furnishings (benches, trash receptacles, bollards, etc.)</li><li>■ Repair flood damage: silt clean-up, culvert clean-out, etc.</li><li>■ Patching/minor regrading/concrete panel replacement</li><li>■ Snow removal</li></ul>	Volunteers, Trail Maintenance Coordinator/Committee
Seasonal	<ul style="list-style-type: none"><li>■ Planting/pruning/beautification</li><li>■ Culvert clean-out</li><li>■ Installation/removal of seasonal signage</li></ul>	Volunteers, Trail Maintenance Coordinator/Committee
Annually	<ul style="list-style-type: none"><li>■ Surface evaluation to determine need for patching paved trails and re-grading soft surface trails</li></ul> <p>Note: Regular preventive maintenance can extend the life of an asphalt trail by a decade or more.</p>	Coordinator/Committee

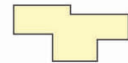




<b>Frequency</b>	<b>Trail Maintenance Tasks</b>	<b>Responsible Party</b>
5-year	<ul style="list-style-type: none"> <li>■ Repaint or repair trash receptacles, benches, signs and other trail amenities, if necessary</li> <li>■ Seal-coat asphalt trails</li> </ul>	Volunteers, Trail Maintenance Coordinator/Committee
10-20 years	<ul style="list-style-type: none"> <li>■ Resurface/regrade/restripe trail</li> </ul>	Hired Contractor, Trail Maintenance Coordinator/Committee, Volunteers
20+ years	<ul style="list-style-type: none"> <li>■ Replace/reconstruct trail as needed</li> </ul>	Hired Contractor, Trail Maintenance Coordinator/Committee, Volunteers



REGIONAL JURISDICTION MAP



LEGEND

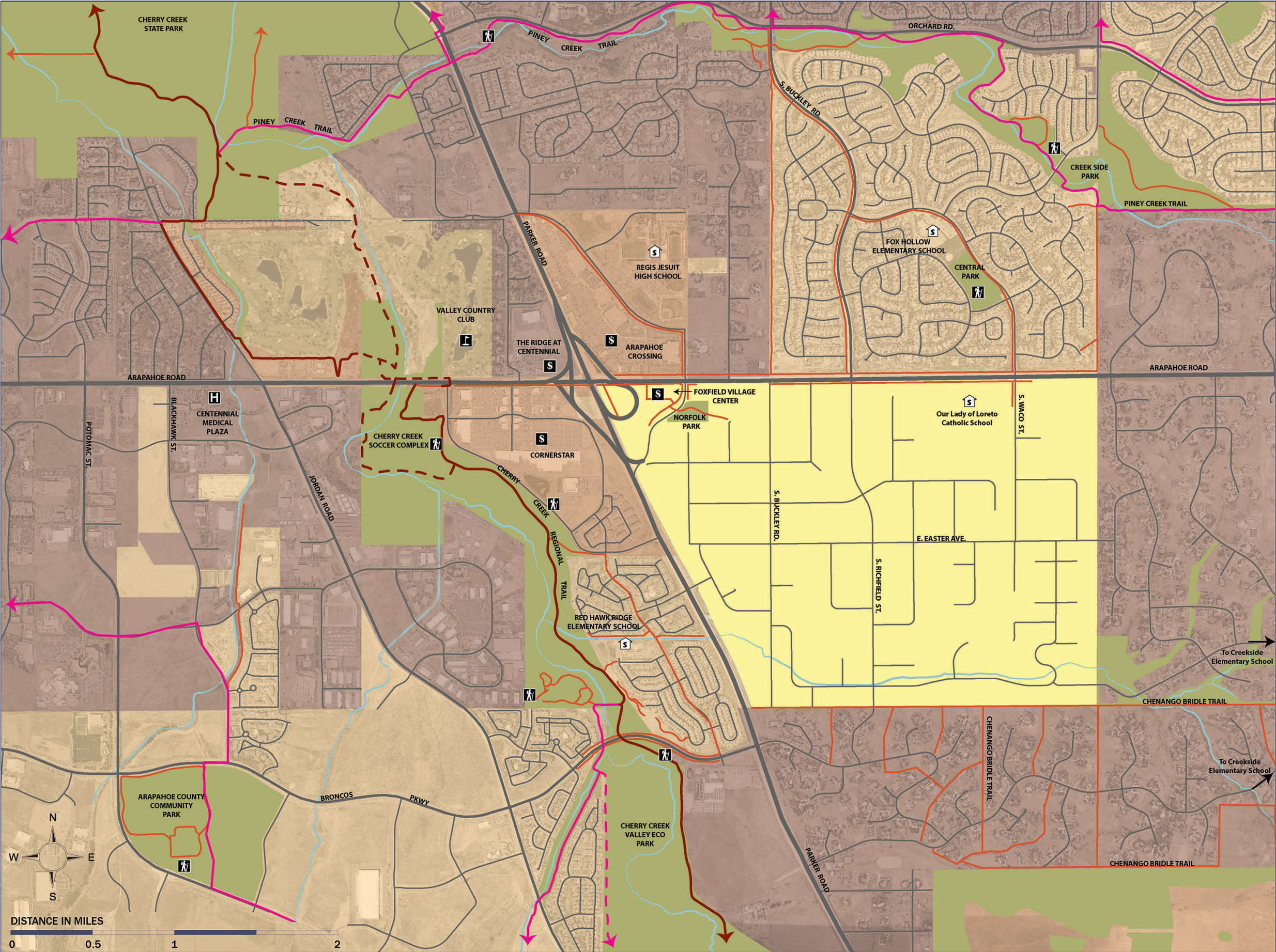
-  Town of Foxfield
-  City of Centennial
-  City of Aurora
-  Unincorporated Arapahoe County
-  Adjacent Neighborhood & HOA Boundary



Map Disclaimer:  
This map is intended for graphic display purposes only and is not accurate to survey standards. The Town of Foxfield and McCool Development Solutions does not assume liability for any inaccuracies contained herein.



REGIONAL TRAIL NETWORK MAP



LEGEND

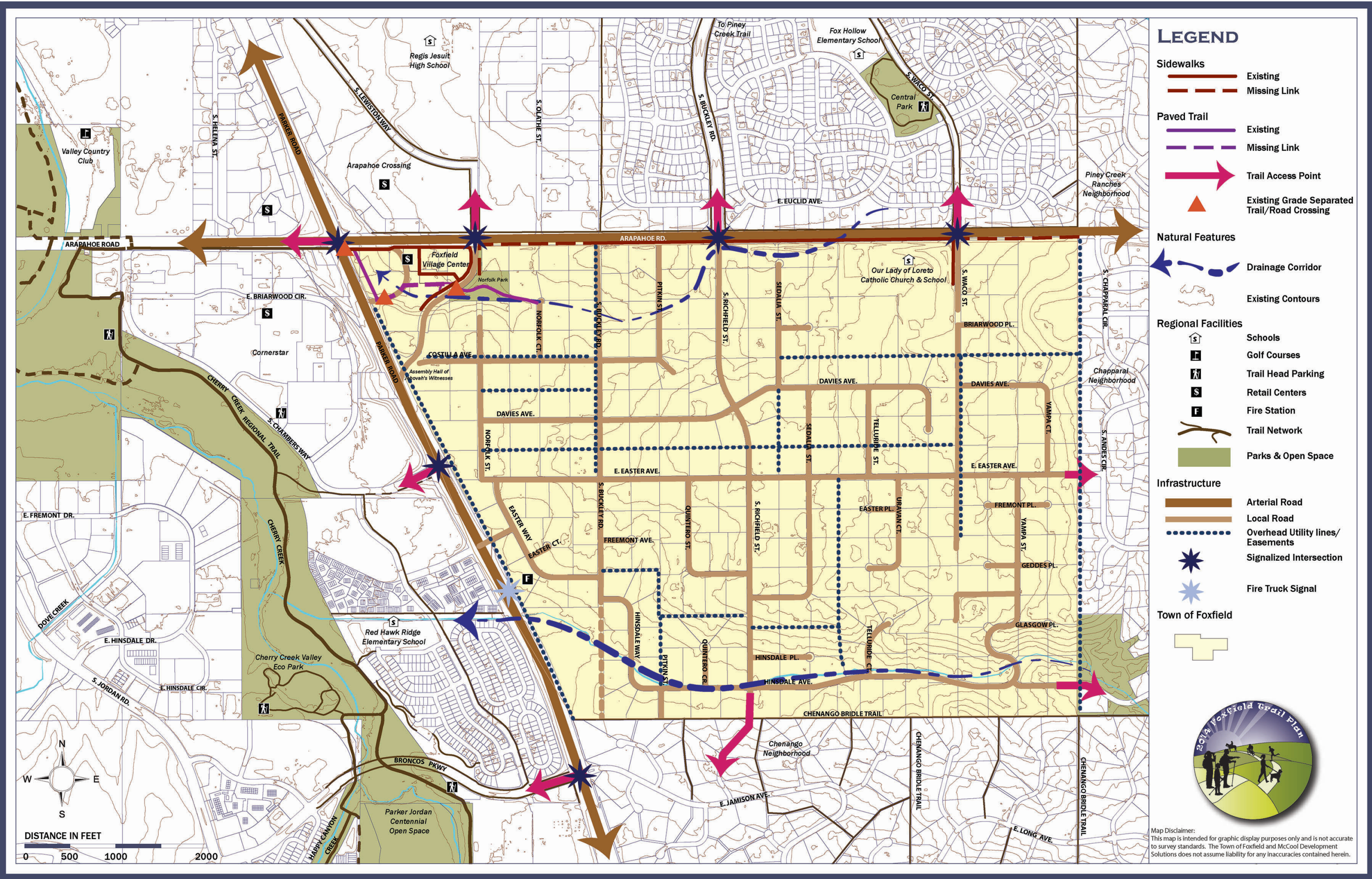
- Regional Trail Corridor
- Existing
  - Future
- Primary Trail Corridor
- Existing
- Neighborhood Trails/Sidewalks
- Existing
- Jurisdictions
- Town of Foxfield
  - City of Centennial
  - City of Aurora
  - Unincorporated Arapahoe County
- Facilities
- School
  - Golf Course
  - Trail Head Parking
  - Retail Center
  - Health Care Complex
  - Parks & Open Space



Map Disclaimer:  
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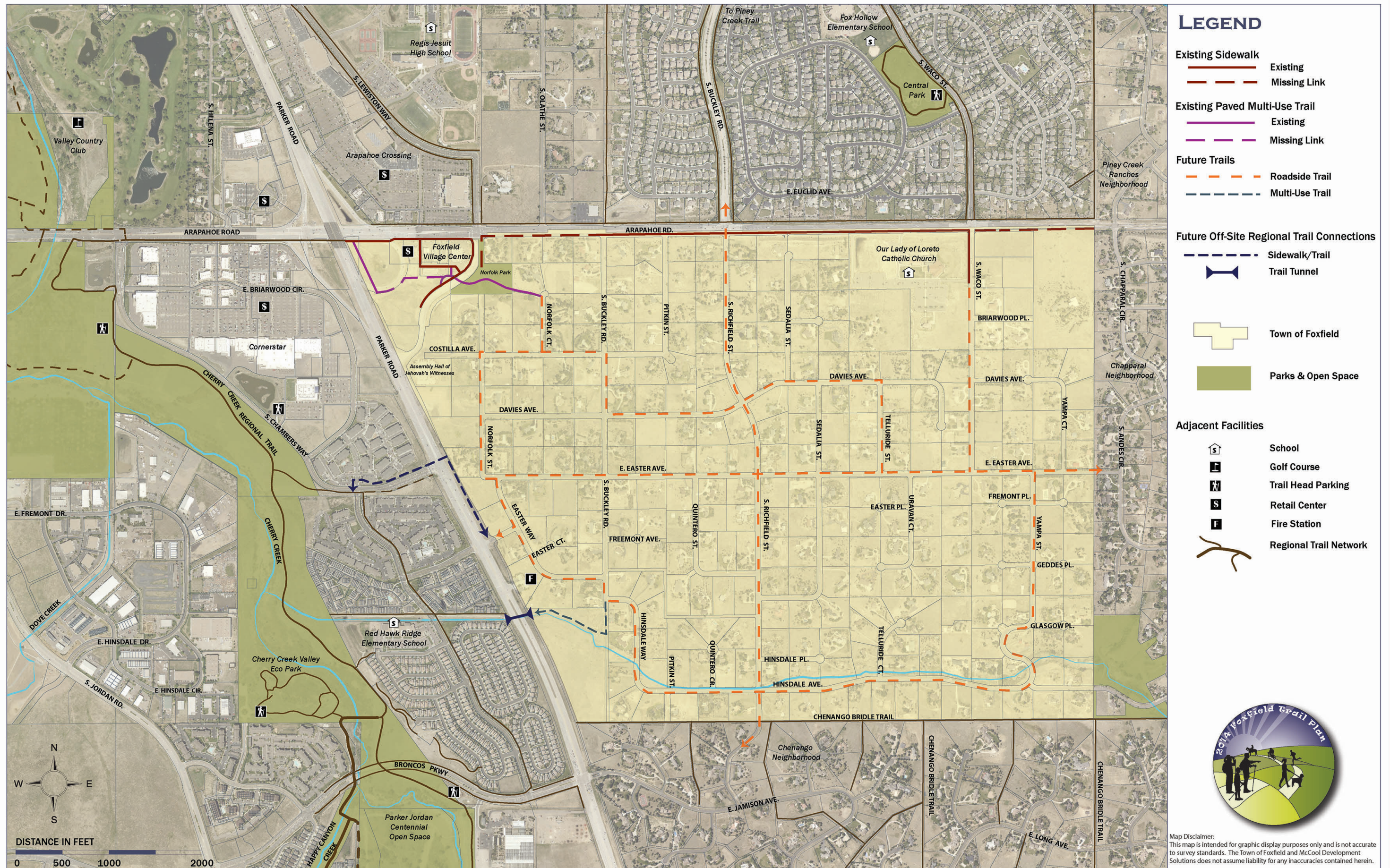


EXISTING CONDITIONS MAP





## FOXFIELD TRAIL NETWORK MAP





## **Appendix A.**

### **Foxfield Trail Survey Summary**



## TRAIL PLAN SURVEY REPORT

January 30, 2014



### Introduction & Methodology

The Town of Foxfield Trail Plan Survey is part of a larger Foxfield Trail Master Plan project funded by an Arapahoe County Open Space Planning Grant. The Master Plan will address the trail and access needs of all Foxfield community members, including pedestrians, cyclists, runners/joggers, and equestrians. The goal of the project is to design a trail network/system in and around the Town of Foxfield that will connect to the adjacent trail systems including the Cherry Creek and Piney Creek trails. Ultimately this will provide safer access to nearby retail centers and greater opportunity to enjoy Colorado's great outdoors.

The Trail Plan Survey was designed to help the Town better understand who uses trails, what types of improvements are most needed and most supported, and how the Town might be able to make them a reality.

In January 2014 the Town of Foxfield, in conjunction with the Planning Team, administered the Trail Plan Survey. The survey was distributed to Foxfield residents as well as citizens of nearby Home Owners Associations (HOA) that may present opportunities for future collaboration on regional trail planning issues. The survey was distributed via both a paper copy and an online version, in order to ensure wide access to the survey by all residents. The survey link was posted on the Town's local notice boards and was emailed out to the Town's email list-serve and adjacent HOA Boards of Chenango, Chapparral and Piney Creek.

The Planning Team received a total of 185 survey responses. The bulk of these surveys were completed online, with 3 paper surveys completed and returned by mail. The Planning Team entered results from the paper surveys into the online survey once it had closed to the public, to allow easy compilation and analysis of all results in a consistent fashion. Final analysis of all responses is provided below and will provide a foundation for the Foxfield Trail Master Plan project.

### Summary of Results

Overall, results from the Trail Plan Survey indicate that the majority of survey respondents are in favor of the Town of Foxfield creating new trails, as well as improving and linking existing trails. Respondents believe that better connectivity with regional trails, particularly the Cherry Creek system, and connectivity with commercial areas would increase trail usage throughout Foxfield. Respondents also desire separate, dedicated walking paths and sidewalks to allow for safe passage within the town.

The remainder of this report details survey responses to questions about how trails are currently used in Foxfield, changes that would increase trail usage, potential future trail projects and improvements, and opinions on funding and maintaining trails. Survey respondents were also provided the opportunity to leave open-ended comments detailing any issues of importance or concern related to trail projects in Foxfield.

The full set of charts and graphs corresponding to the Trail Plan Survey questions may be found in the Appendix to this document.

### General Description of Survey Respondents

The majority of survey respondents (58%) were Foxfield residents; 42% were residents of neighboring communities. Only one respondent indicated that he or she owned a business in town. The majority of respondents (74%) fell in the 35-64 year age bracket. Twenty three percent (23%) indicated they were 65 years or older, and a small percentage (roughly 3%) were 18-34 years of age. The age 17 years or younger age bracket was not represented in this survey.

### Trail Use by Survey Respondents

The Planning Team sought to form an overall picture of how trails in Foxfield are currently being used. The Trail Survey posed a series of questions regarding frequency of trail use, types of activities pursued on local trails, general purposes for trail use, and changes that might contribute to increased trail usage.

#### *Frequency of Trail Use*

The majority of survey respondents (70%) use local and regional trails or pathways on at least a weekly basis. Of these, 32% use trails daily, and 38% do so on a weekly basis. A smaller percentage use trails at least once a month (11%) or a few times a year (12%). Six percent (6%) of survey respondents never use local or regional trails. (Figure 1)

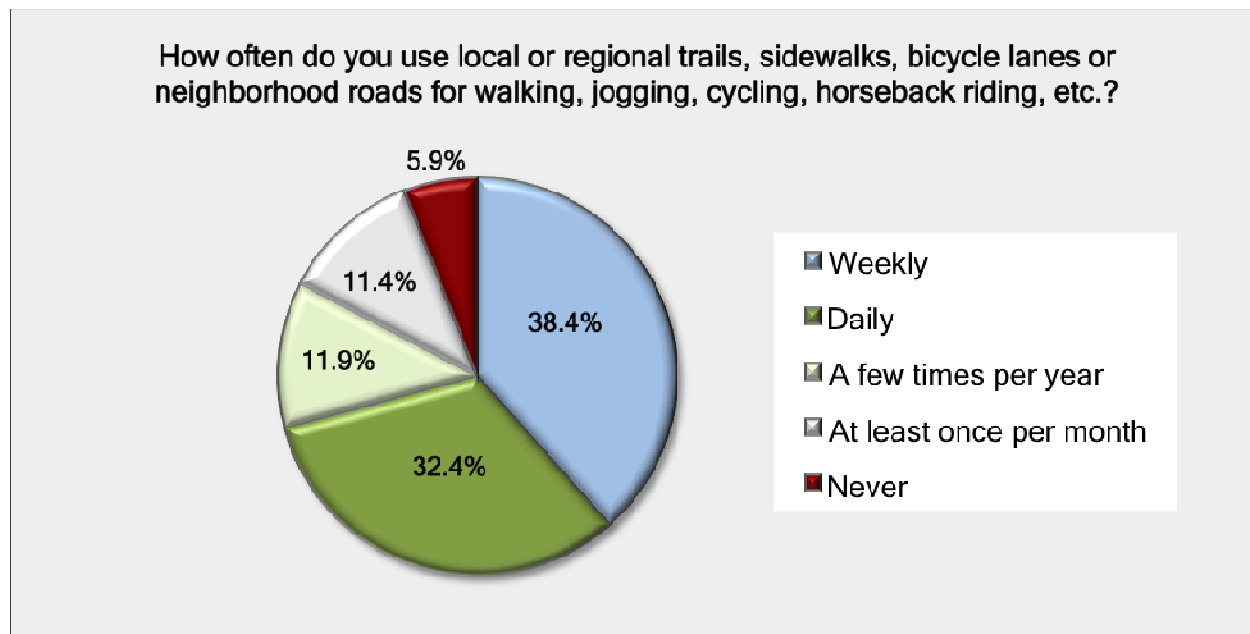
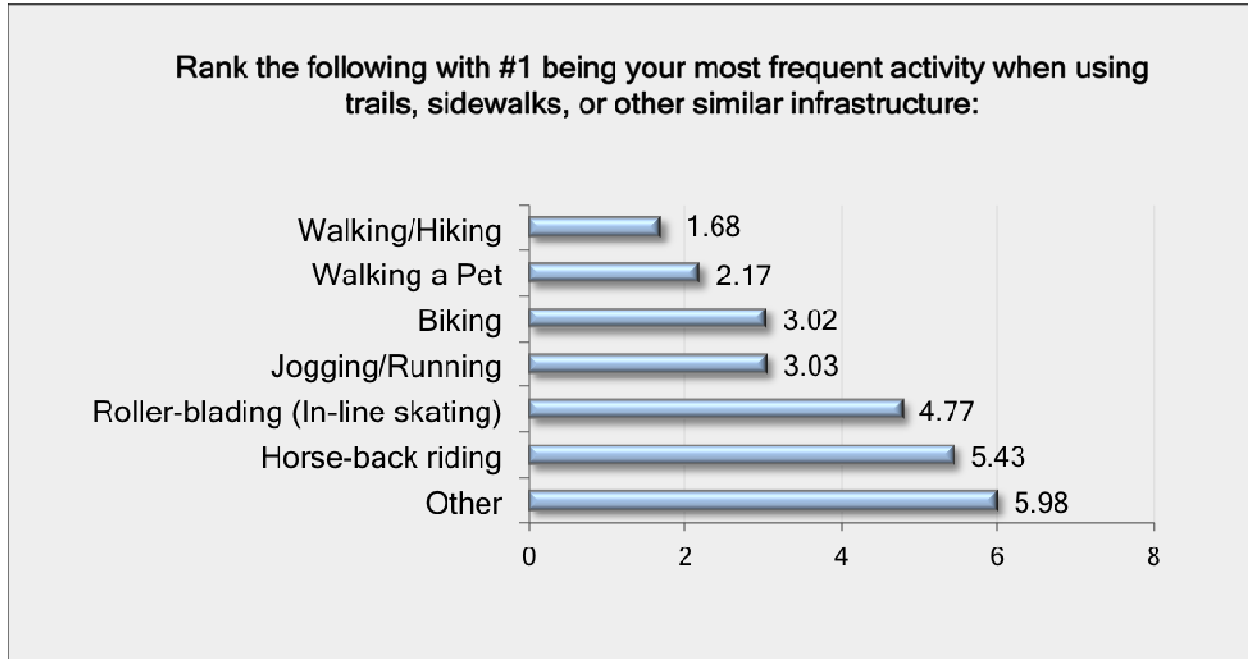


Figure 1



### ***Types of Trail Activities***

Survey respondents ranked **walking/hiking** as the most frequent trail activity, followed by walking a pet. Biking and jogging/running were ranked very closely in the middle. Roller-blading and horseback riding were ranked as relatively less frequent activities. (Figure 2)



**Figure 2**

### ***Purpose for Trail Use***

The majority of trail users (65%) indicated that their main purpose in using trails or other infrastructure is for **exercise and fitness**. Roughly 23% use trails for the purpose of leisure. A much smaller percentage use trails for nature watching (5%) and for transportation or commuting (2%). Roughly 5% of respondents said they do not use trails. Additional comments by survey respondents mentioned trail use to enjoy scenic beauty and the outdoors, for shopping, walking to school, and walking dogs. (Figure 3)

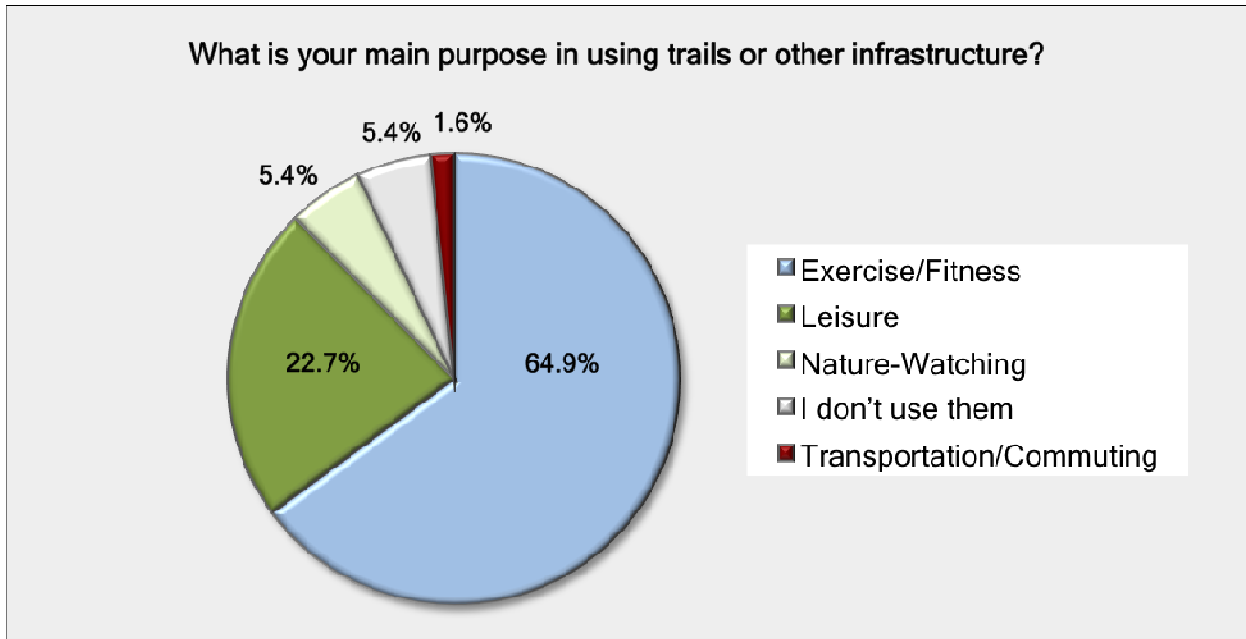


Figure 3

### Incentives for Increased Trail Usage

A majority of survey respondents indicated that **better connections to regional trails** (64%), such as Cherry Creek, and having **separate trails** (60%) would act as an incentive for them to use trails more often in Foxfield. Better connections to nearby commercial areas would motivate roughly 30% of respondents to use trails. Twenty five percent (25%) said they would use trails more often if there were more sidewalks in Foxfield.

Fewer respondents selected, as part of their top 3 options: better connections to nearby neighborhoods (15%), bicycle lanes/routes (14%), safer street crossings (12%), and better signage (9%). (Figure 4)

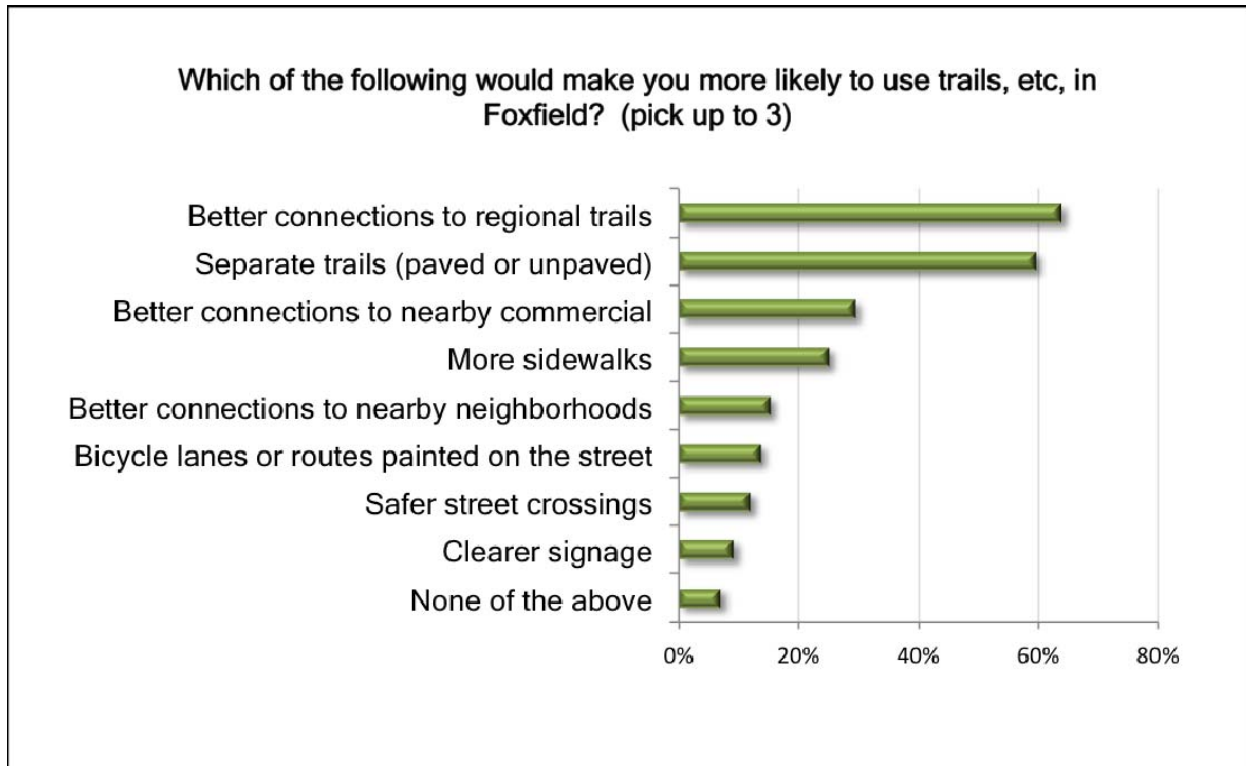


Figure 4

### Significance of Trail System

Survey respondents were asked how strongly they agreed with statements regarding the value of trails in general. (Figure 5) A majority indicated that they **strongly agreed** with the following:

- Trails promote a healthy lifestyle and high quality of life (81%)
- Trails allow users to connect with nature (56%)

A majority **moderately to strongly agreed** with the following:

- Trails are central to recreation in the Southeast Metro area (45% strong; 30% moderate)
- Trails are an important tool in addressing childhood obesity (35% strong; 28% moderate)
- Trails provide an important venue for learning about the ecology and nature of the Southeast Metro area (23% strong; 43% moderate)

A majority were at minimum **neutral or moderately agreed** with the following statements:

- Trail tourism is economically significant to the Southeast Metro area (26% moderate; 39% neutral)
- Trails are important for transportation in the Southeast Metro area (34% moderate; 31% neutral)
- Trails provide an important venue for learning about the history and culture of the Southeast Metro area (34% moderate; 37% neutral)



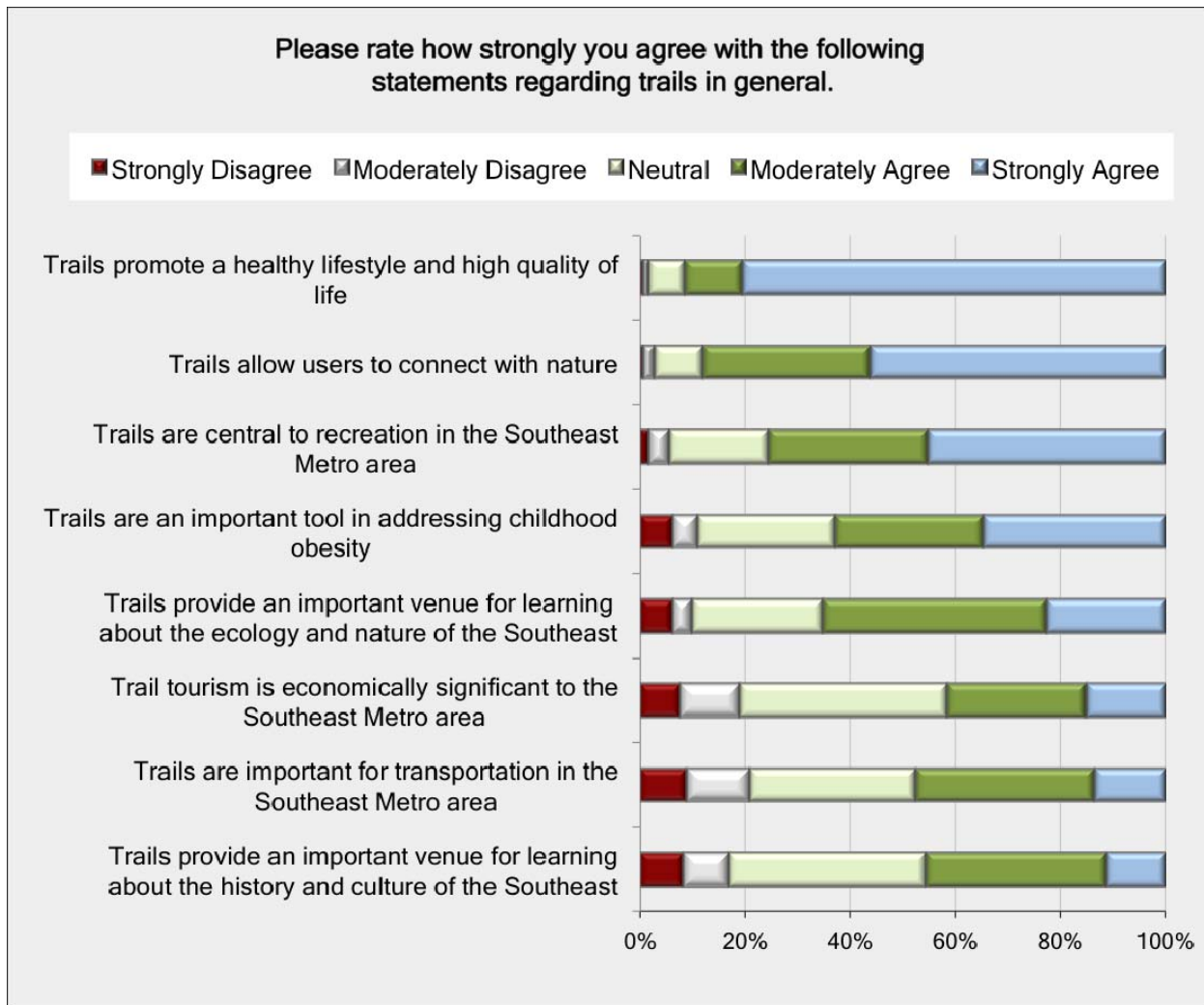


Figure 5

### Possible Trail Projects and Improvements

The Planning Team asked a series of survey questions designed to elicit whether survey respondents support the creation of new trails and trail connections, as well as which particular trail projects are considered a priority by respondents.

#### *New Trails & Connections*

A strong majority of approximately 80% of survey question respondents said they would support the Town of Foxfield in creating new trails or trail connections. Roughly 14% were not sure, and 6% would not support trail creation. (Figure 6)

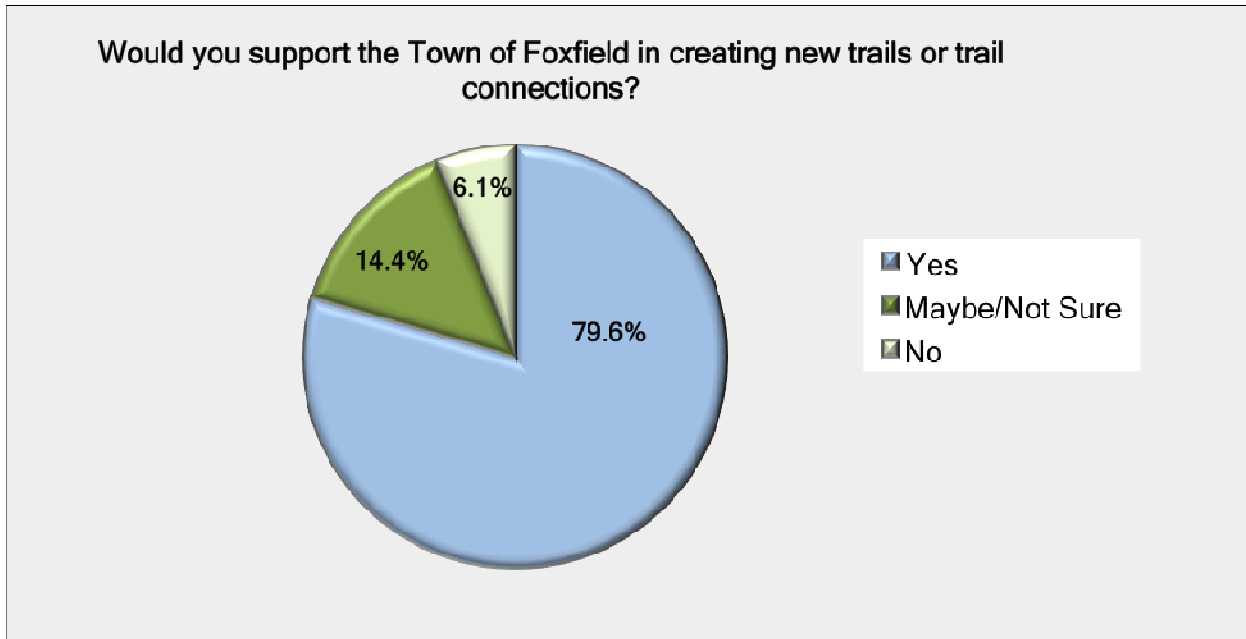


Figure 6

#### *Selecting “One Project”*

The Trail Plan Survey next asked respondents to identify **one trail project or improvement** for Foxfield that they would request, including its location. This was an open-ended question in which respondents wrote in their answer. One hundred and twenty four (124) respondents provided a response to this question (See Appendix, Question 6 for full list of actual responses).

The top open-ended responses to this question included the following, ranked by frequency of response:

1. **Connection to Cherry Creek trail system**
2. **Installation of sidewalks and/or walking trails**
3. **Safe road crossings (particularly at Parker Road)**
4. **Connection to commercial areas (Cornerstar, Arapahoe Crossing etc.)**
5. **Regional trail connections**
6. **Dedicated bike paths**
7. **Finishing and clearing original trails**
8. **Dedicated horse trails**
9. **Miscellaneous requests (noted by 3 or fewer respondents)**

Comments regarding the top response, creating a connection to the Cherry Creek trail system, focused on the need for either safe road crossings or avoiding crossing busy roads altogether. Parker Road and Arapahoe Road were identified as high traffic areas that currently prohibit safe connections to both the Cherry Creek path and commercial shopping areas (e.g., Cornerstar) in Foxfield. In addition to connecting to the Cherry Creek system, respondents also discussed the desire to expand and link off-road trails to surrounding areas, such as Chenango and Chapparral.

Another top response focused on the need for sidewalks as well as off-road walking trails throughout Foxfield. Respondents discussed the need to walk safely through neighborhoods, particularly for students walking to school and for pedestrians in general. Responses indicated that residents feel current options for walking are dangerous to pedestrians. A sub-set of responses focused on the desire for safer jogging areas that connect to existing trails.

Some residents requested that the Town focus first on finishing the set of trails that were already started, included in the road bonds, and designated in the Town's existing Master Plan. Respondents noted that there are original bridle/walking trails that need to be cleared of fences and vegetation.

A smaller number of respondents indicated the desire for trails and paths dedicated solely to biking or horse trails. Comments acknowledged a need for strict rules for biking activity, such as speed and designated sides of the trail.

Comments in the miscellaneous category included: requests for trail amenities such as benches, trash containers, signs, and dog waste disposal; traffic-related requests, such as speed bumps, sealed roads, and addressing church traffic; maintenance requests, such as weed control and graffiti removal; and the desire for a central town park and community space/gardens. A few respondents indicated that they do not think the Town should install trails at all.

### Funding Options for Trails

The Planning Team posed two questions designed to elicit respondents' opinions on how the Town should finance and implement trail projects and improvements.

#### *Town Financing of Trail Projects*

Respondents were asked to indicate any and all methods that the Town of Foxfield should employ to finance trail projects or improvements. Results reported here were analyzed based solely on responses from residents of Foxfield (Figure 7). A strong majority of residents (86%) were in favor of **grants** as a method for financing trails. A majority of 64% also believed the Town should use **donations** as a financing option. Only 24% of respondents thought that taxes should be used for this purpose.

Of those respondents who marked "Other", several suggested allocating a portion of **current** tax revenues to support trail development. Additional suggestions for financing trails included:

- Community events & fundraisers
- Money toward actual easement acquisition and construction
- Lottery funds
- HOAs
- Sales taxes

In addition, some comments in the "Other" category reiterated opposition to new taxes and concern regarding high taxes and prohibitive cost of trail maintenance, noting that the Town does not have a broad enough base to support trail improvements.



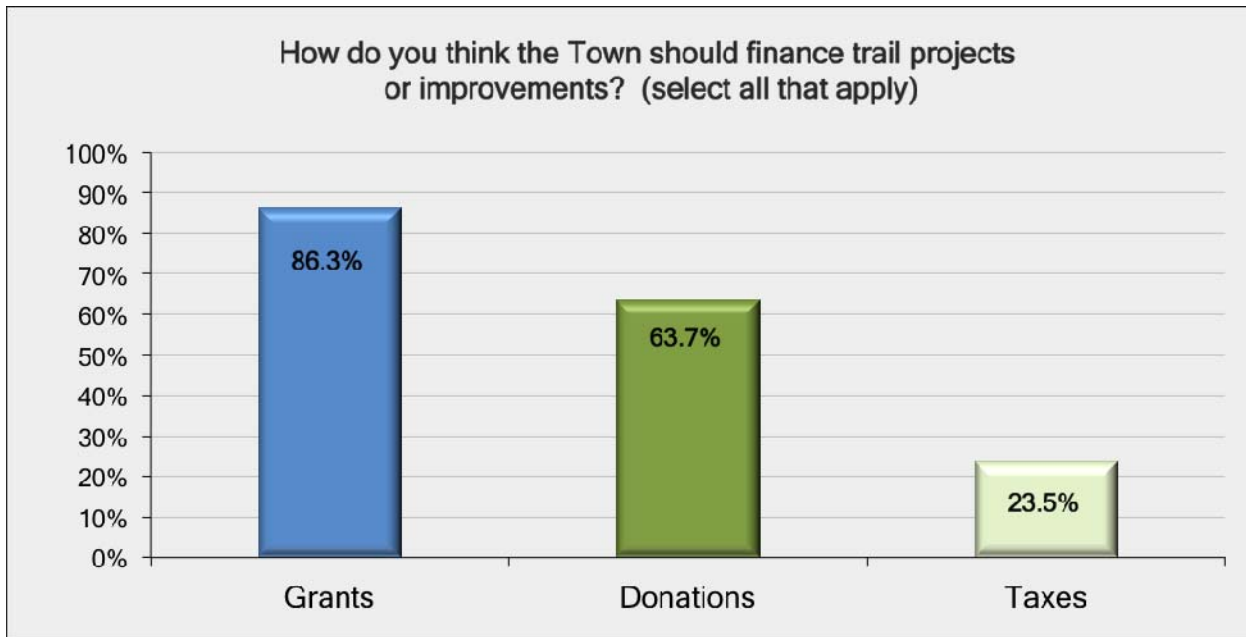


Figure 7

#### *Citizen Support for Trail Projects*

Roughly 85% of question respondents said they would be willing to volunteer to help improve Foxfield's trail system. Thirty-eight percent (38%) would make donations of money or materials, and 28% would allow for trail easements on their property to improve local trails. Several survey respondents indicated that they already had trail easements in place on their properties. A few qualified their answers by indicating that they would allow an easement depending on where it would be located on their property. (Figure 8)

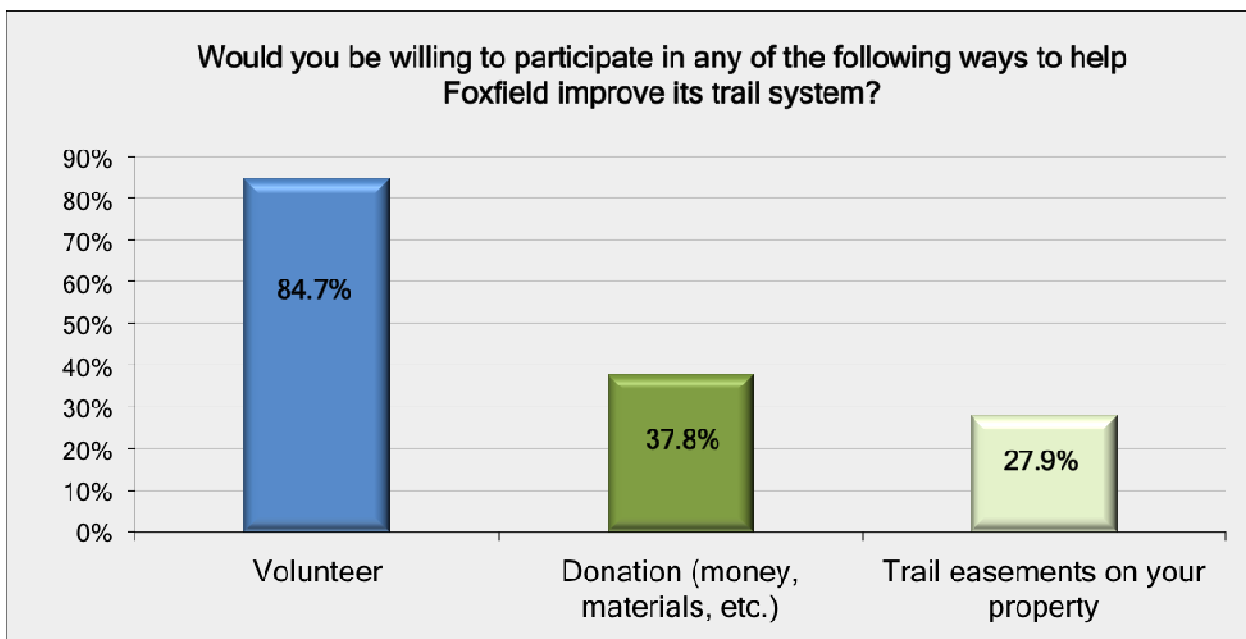


Figure 8

## Final Comments

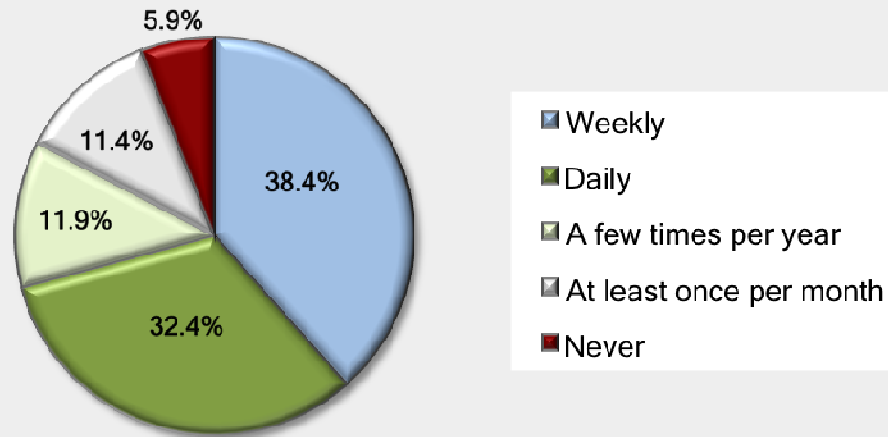
Survey respondents were given the opportunity to leave final comments on the survey to ensure that all issues related to trail creation and improvements were addressed. (See Appendix, Question 10 for full list of actual responses). Forty-four (44) survey respondents chose to leave additional final comments. Many of the comments indicated general support for trails and trail improvements. Survey respondents indicated that trails and sidewalks would increase pedestrian safety in Foxfield. Residents also noted that trail improvements should address road crossings and focus on connecting existing trails to Cherry Creek trail, local businesses, and surrounding areas. A few requested that a map of existing trails be included in any future surveys or discussions related to trail improvements.

Respondents also used the final comments section to express specific concerns in relation to trails and trail improvements in Foxfield. Funding and maintenance costs, as well as impacts from attracting non-residents to Foxfield area, were key concerns. Respondents pointed out that trails would incur maintenance costs to the Town, and expressed a desire to avoid fees and new taxes to fund the trails. Several indicated that it is unnecessary to expand the trail system in Foxfield; one of these respondents suggested that restoring the original trails should take priority over expansion. Some noted that an increase in non-residents visiting the area might relate to potential increases in crime, loss of privacy, and increased traffic. Others expressed a desire to maintain the unique rural character of town.

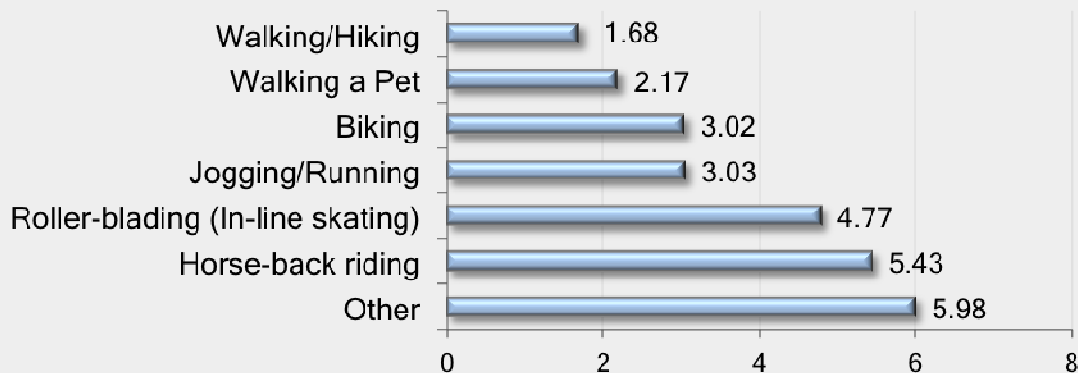
**Appendix:**  
**Survey Responses**  
**in Chart, Graph, and Table Format**

**Question 1**

How often do you use local or regional trails, sidewalks, bicycle lanes or neighborhood roads for walking, jogging, cycling, horseback riding, etc.?

**Question 2**

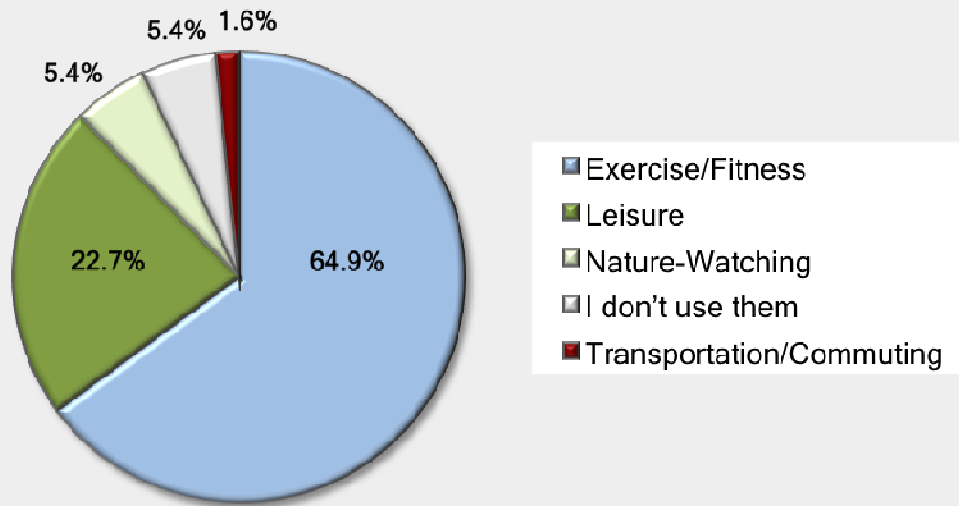
Rank the following with #1 being your most frequent activity when using trails, sidewalks, or other similar infrastructure:





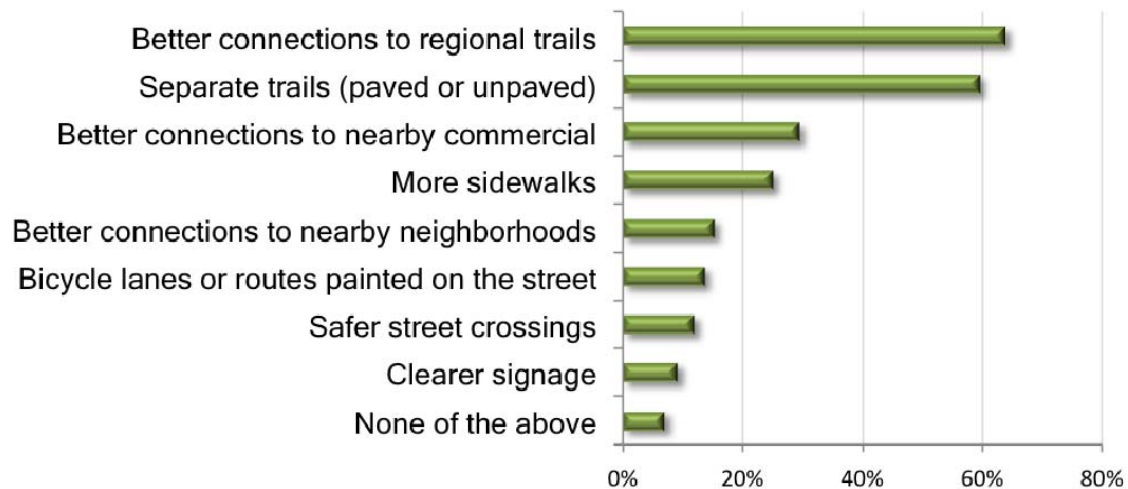
## Question 3

What is your main purpose in using trails or other infrastructure?

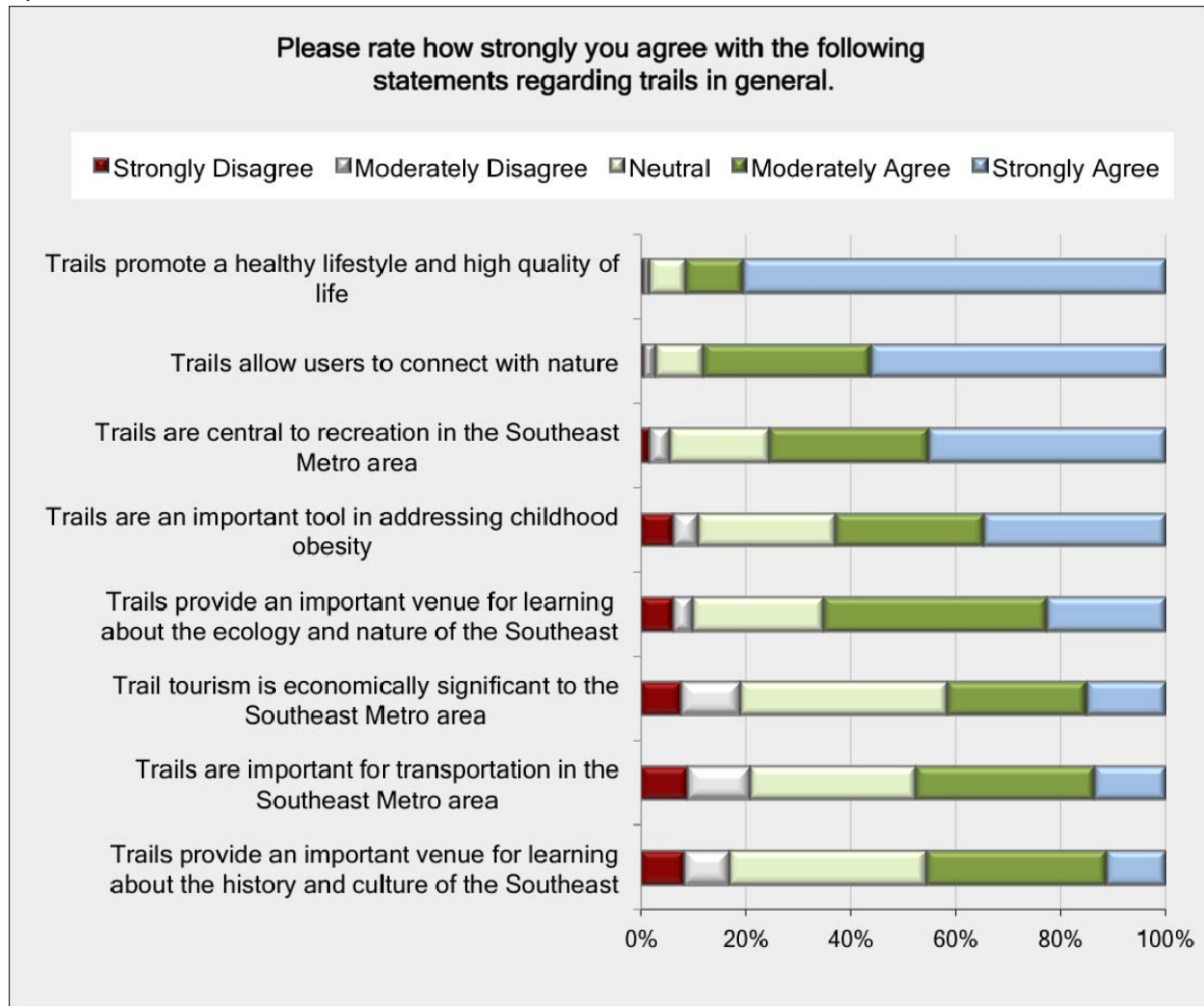


## Question 4

Which of the following would make you more likely to use trails, etc, in Foxfield? (pick up to 3)



## Question 5



## Question 6

**Responses to Open-Ended Question: "If you could request one trail project or improvement for Foxfield, what would it be and where?"**

**Connect to Cherry Creek Trails**

More horse/dog walking trails in neighborhood and connecting to Cherry Creek Trails.

Connection to Cherry Creek trail

Connect to cherry creek trail

Connection to cherry creek park without having to cross busy road at traffic level

Safe connection to the Cherry Creek Trail System without having to directly cross busy Parker Road/Arapahoe Road.

Connections to the Cherry Creek trails for safe, traffic-free horseback riding and dog walking.

Connections to neighboring areas: North to power line right of way, South to Chenango system, West to Cherry Creek system

Expand and link trails to surrounding area, including Chenango, Chapparal, Cherry Creek system, etc.

Connection to cherry creek trail system

Connection to Cherry Creek Trail system

Connect to cherry creek trail system

**Responses to Open-Ended Question: "If you could request one trail project or improvement for Foxfield, what would it be and where?"**

It would be nice to be able to get from Chapparral to Foxfield to Cherry Creek State park and other trails without ever having to go on the roads. It would be nice to connect the High School, Liberty, and Creekside so children can ride their bikes to school on trails without any (or minimal) car traffic.

Safer routes for pedestrians/bikers then just using roads, i.e. sidewalks or trails (paved or unpaved). Connecting somehow to the Cherry Creek trails would be a bonus.

A paved trail that connects through Foxfield, Chapparral, and Chenango to cherry creek trail behind Tagawa gardens.

Trail connection to cherry creek reservoir

Trail adjacent to Easter that eventually links to Chenango and over to Jamison so that you can cross Parker and access the Cherry Creek Bike path

Expand and link trails to surrounding area, including Chenango, Chapparral, Cherry Creek system, etc.

Better connection to Cherry Creek State Park

Passage under or over Parker road and into the cherry creek trail

Trail that connects to Cherry a Creek trail.

A trail that is paved from fox field connecting to the cherry creek trail.

Connection to larger trails and to Cherry Creek State Park

Foxfield to Cherry Creek Trail system without using roads

Most important one is a pathway under Parker Road so can access Cherry Creek trail system.

Primarily we need a turn in built for the church @ the end of Costilla, also a trail/ bridge/ tunnel to get from Foxfield across parker to Cornerstar or nearby to access cherry creek trail

Town access to Cherry Creek Trail System

Connect to Cherry Creek trails

An easier way to get to Cherry Creek State Park - Crossing Arapaho road to get to Orchard Trail

Provide a paved trail that connects the community to Cherry Creek trail system and Town of Foxfield/Arapahoe Crossing shopping centers

Trail connection to shopping centers and cherry creek bike trail

Connect with Cherry Creek

Make so we could access cherry creak trail from Foxfield

To continue the trail over to Cherry Creek

A connection to the Cherry Creek system

Connect to CornerStar & Connect to our retail area. It would be a boost if we could also connect to Cheery Creek trail and be able to ride all the way to Denver.

**Installation of Sidewalks/Walking Trails**

Pathways on side of road

It would be great to have a trail or sidewalk somewhere in Foxfield.

Sidewalks should be added just to be able to walk safely around the neighborhood.

Anything off-street that is accessible to the maximum number of residents possible with the shortest walking distance, i.e., something central such as along Richfield, Easter, or Hinsdale

Trails on the interior limits of Foxfield so residents can walk safely in the Town

A trail in the neighborhood that is not on the roads. I feel it is very dangerous to walk especially on Hinsdale and Richfield with traffic. OR perhaps the best solution would be to add a sidewalk which would also be safer for school children riding the bus.

Side walks

Sidewalks

Safer routes for pedestrians/bikers then just using roads, i.e. sidewalks or trails (paved or unpaved). Connecting somehow to the Cherry Creek trails would be a bonus.

Sidewalks



**Responses to Open-Ended Question: "If you could request one trail project or improvement for Foxfield, what would it be and where?"**

Sidewalk along Arapahoe road going to and from Grandview high school

Sidewalks

Sidewalks

A safe walkway/sidewalk/trail from S. Buckley Road to Lewiston so that the Regis Jesuit High School students in Foxfield can walk there safely.

Either a marked pedestrian lane along the side of the roads or a separate trail in the ROW.

Sidewalk on Arapahoe, west of Buckley to shopping center and light at Lewiston

Walking/hiking throughout Foxfield.

Walking trails that are easily accesses to residents and hook into other trails and commercial development.

Walking trails. Not sure where.

Walking trails and trees

It would be nice to have trails that are away from the streets and intersections.

Separate trails so I don't have to walk on the street and worry about cars

A loop like off road (due to traffic) soft trail

Trail for dog walkers and/or pedestrians only - no bikes. Easiest would be to follow the roads around the town perimeter

Bike and walking trail.

Safer jogging areas

A running/walking trail that was off-road and more secluded.

Jogging trail that connects to Cherry Creek Trail

Runner friendly

Dedicated running and bike trails - also strict rules for biking activity; speed, side of the trail...

**Safe Road Crossings (Particularly at Parker Road)**

Safe connection to cross Parker

Safe connection to the Cherry Creek Trail System without having to directly cross busy Parker Road/Arapahoe Road.

Crossing Parker Road safely

Trail adjacent to Easter that eventually links to Chenango and over to Jamison so that you can cross Parker and access the Cherry Creek Bike path

Passage under or over Parker road and into the cherry creek trail

Most important one is a pathway under Parker Road so can access Cherry Creek trail system.

Better crossings for Parker and Arapahoe roads to other trails

Safe access to the west side of Parker Rd.

Trail/road crossings = at grade

Primarily we need a turn in built for the church @ the end of Costilla, also a trail/ bridge/ tunnel to get from Foxfield across parker to cornerstar or nearby to access cherry creek trail

An easier way to get to Cherry Creek State Park - Crossing Arapaho road to get to Orchard Trail

A trail to safely cross Parker Rd. @ Arapaho & Parker Road.

Connect to the west side of Parker Road via a tunnel or bridge.

Would like the trails to all connect with little crossing of main roads for safety. Plus I am not sure of all the trails we currently have to promote and provide information on these regularly.

**Connection to Commercial Areas (Cornerstar, Arapahoe Crossings, etc)**

Safe connection to commercial areas king soopers arap crossing and Cheddars area

Connect to commercial

Connection to cornerstar

Primarily we need a turn in built for the church @ the end of Costilla, also a trail/ bridge/ tunnel to get from Foxfield across parker to cornerstar or nearby to access cherry creek trail

**Responses to Open-Ended Question: "If you could request one trail project or improvement for Foxfield, what would it be and where?"**

Sidewalk on Arapahoe, west of Buckley to shopping center and light at Lewiston

Access to commercial area

Safer trail to shopping center

Bike/walk trail to cornerstar from foxfield

Connect to CornerStar & Connect to our retail area. It would be a boost if we could also connect to Cheery Creek trail and be able to ride all the way to Denver..

Provide a paved trail that connects the community to Cherry Creek trail system and Town of Foxfield/Arapahoe Crossing shopping centers

Trail connection to shopping centers and cherry creek bike trail

Connect thru to the trail at Walgreens.

Finish connecting the trail on the south side of the shopping center.

**Regional Trail Connections**

Connections to neighboring areas: North to power line right of way; South to Chenango system; West to Cherry Creek system

Obtain trail easement through Worsham property in order to provide future connectivity to regional trails.

More trails like Chenango.

Expand and link trails to surrounding area, including Chenango, Chapparral, Cherry Creek system, etc.

Connect open space property west under Parker Rd. Trail connections east, south & north.

Connect to other dedicated trails.

It would be nice to be able to get from Chapparral to Foxfield to Cherry Creek State park and other trails without ever having to go on the roads. It would be nice to connect the High School, Liberty, and Creekside so children can ride their bikes to school on trails without any (or minimal) car traffic.

Throughout chaparral, chenango and foxfield, off road

Trail connecting Foxfield to the western trails

Off-street ease of movement between Foxfield, Chapparral & Chennago.

Access to cottonwood trail

**Dedicated Bike Paths**

Bike and walking trail.

Trails that connect with the other bike trails that are nearby.

Better bike paths

Bike trails through the neighborhood

Separate trails that connect with other biking opportunities

Dedicated running and bike trails - also strict rules for biking activity; speed, side of the trail....

Bicycle lane

**Finishing and Clearing Original Trails**

Finish at least a portion of the trail already designated on the Town's Master Plan

"Clear" the ORIGINAL bridle/walking trails. Residents were allowed to fence them off, without providing an alternative.

Clean up trail easements already stated that we have in town and make them accessible again by removing the yucca & fences that people have put in the way

Finish the set of trails that were started.

Finish trails that were to be included with road bonds.

**Dedicated Horse Trails**

More horse/dog walking trails in neighborhood and connecting to Cherry Creek Trails.

Horse only trails within Foxfield

More horse trails

**Responses to Open-Ended Question: "If you could request one trail project or improvement for Foxfield, what would it be and where?"**

We bought in Foxfield due to the dirt roads to ride our horses on - we were promised those trails back when the roads were paved. We need horse trails back.

**Miscellaneous**
*Trail Amenities*

Stations for dog waste disposal

Signs at entry, education about the trail project (mapping or general information about what we are facing)

Benches

Benches and trash containers along the trail

An occasional bench upon which to sit when using the trails

*Traffic-Related*

Speed bumps on Davies Ave. seems like almost all the other streets in Foxfield have some speed control except for E. Davies Ave.

Primarily we need a turn in built for the church @ the end of Costilla, also a trail/ bridge/ tunnel to get from Foxfield across parker to cornerstar or nearby to access cherry creek trail

Seal the roads

Stop cut-through traffic.

Redirect the Jehovah Witness church entrance to ONLY be off of Parker Road. The church traffic is really bad on both Costilla and Norfolk.

*Maintenance and Regulations*

Better weed control--by town or (as last season) by strongly encouraging property owners to take action.

I would like dogs to be on easy. I have had a couple of problems with loose dogs being exercised by owners and scuffling with my leashed dog.

Clean graffiti in tunnel and utilized a graffiti free paint on wall surfaces / maintain and clean drainage areas along trail / snow removal on trail

*Town Park*

Would love a community garden

Town park central to all neighborhoods

Park

*No Trails*

NONE. DO NOT PUT ONE IN. LEADS TO UNWANTED PERSONS IN THE NEIGHBORHOOD, GRAFFITI, TRASH OTHER CRIMINAL RISKS, ETC. JUST LOOK AT CHERRY CREEK TRAIL OR OTHER. IT WILL DEVALUE OUR PROPERTY

None! Keep this community RURAL! The park & trails you have now are not being used.

No trails

*Don't Know*

Don't know

No thoughts

Don't know

Not sure

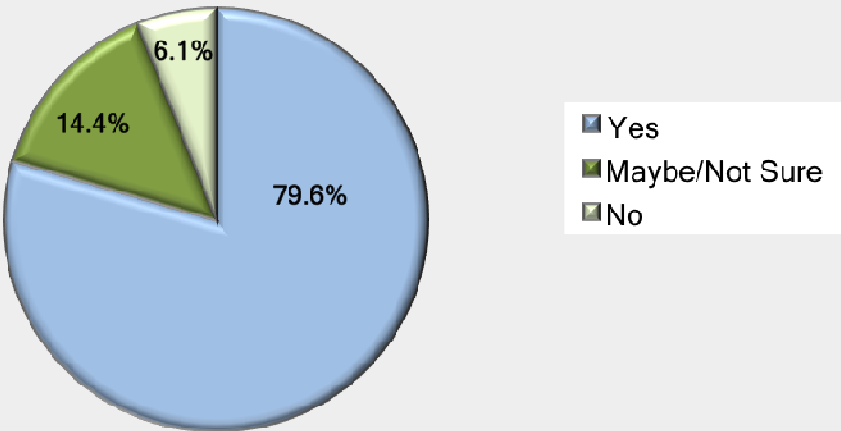
Again, anything would be a vast improvement and much appreciated.

?



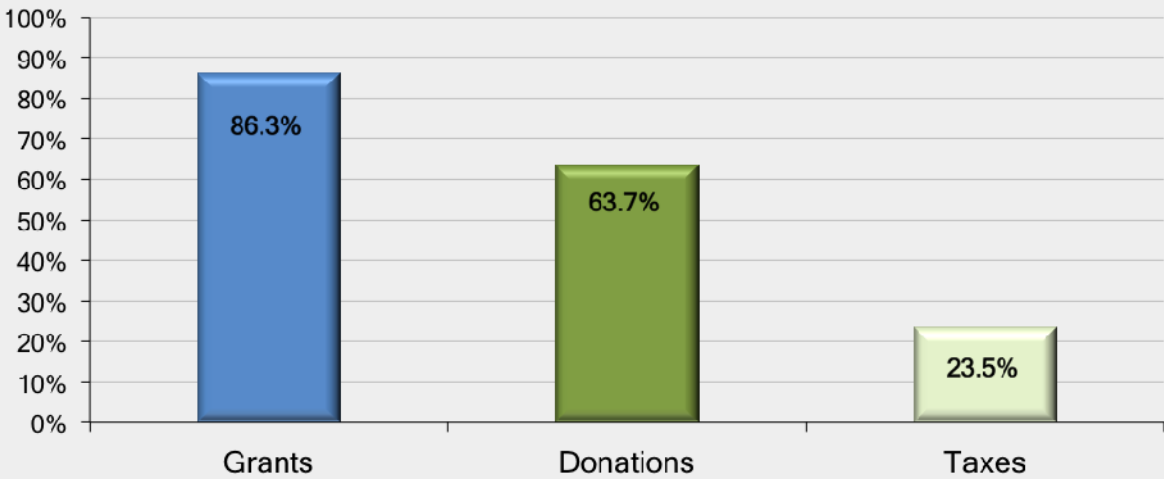
**Question 7**

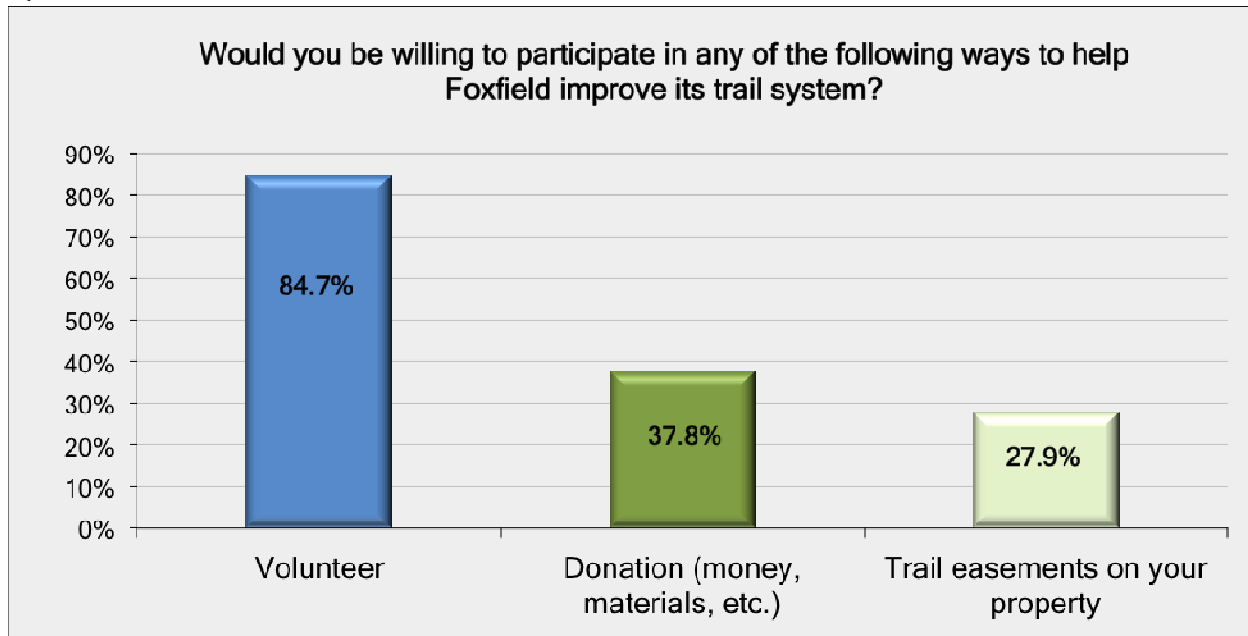
Would you support the Town of Foxfield in creating new trails or trail connections?



**Question 8 (Results from Resident Responses Only)**

How do you think the Town should finance trail projects or improvements? (select all that apply)



**Question 9****Question 10****Responses to Open-Ended Question: "Final Comments – Did we miss anything?"****GENERAL SUPPORT**

Go for it!

I really appreciate you asking residents their opinion and I love the idea and improvements in general.

No, good job.

Great job you guys!! This is a dream of mine and it's so nice to see the torch being carried on. We worked so hard for the first grant. Keep up the good work!!

Great work ... and really appreciate the efforts to help. Super to see people not bitching about things ... and being proactive about things that can enhance our neighborhoods and lives (i.e., trails are healthy in lots of ways). Thank you!

It is high time we do something to get people off their butts and out of their cars.

Thank you.

Excited about the prospect!

The town needs a sidewalk or trail system.

This is a great way to improve the community. There are so many people on the roads and it would be much safer if we provided trails in stead.

Get r done.

I appreciate having the opportunity to have a voice in what happens in our community. We appreciate the boards continued hard work!

**CONCERNS**

Trails bring more trash from visitors and bring strangers into the community that might compromise the safety of especially our kids, but all together any home and his inhabitants & property. Also the loss of privacy is a concern with strangers walking thru your backyard, just ask those folks that live on golf courses. Thanks for keeping Foxfield safe. What is being done about the water holding capacity of the ditch system in Foxfield. After the amazing rainfall we saw last year and the floods caused by it, shouldn't that be addressed sooner than later before anything worse happens? Thanks again.

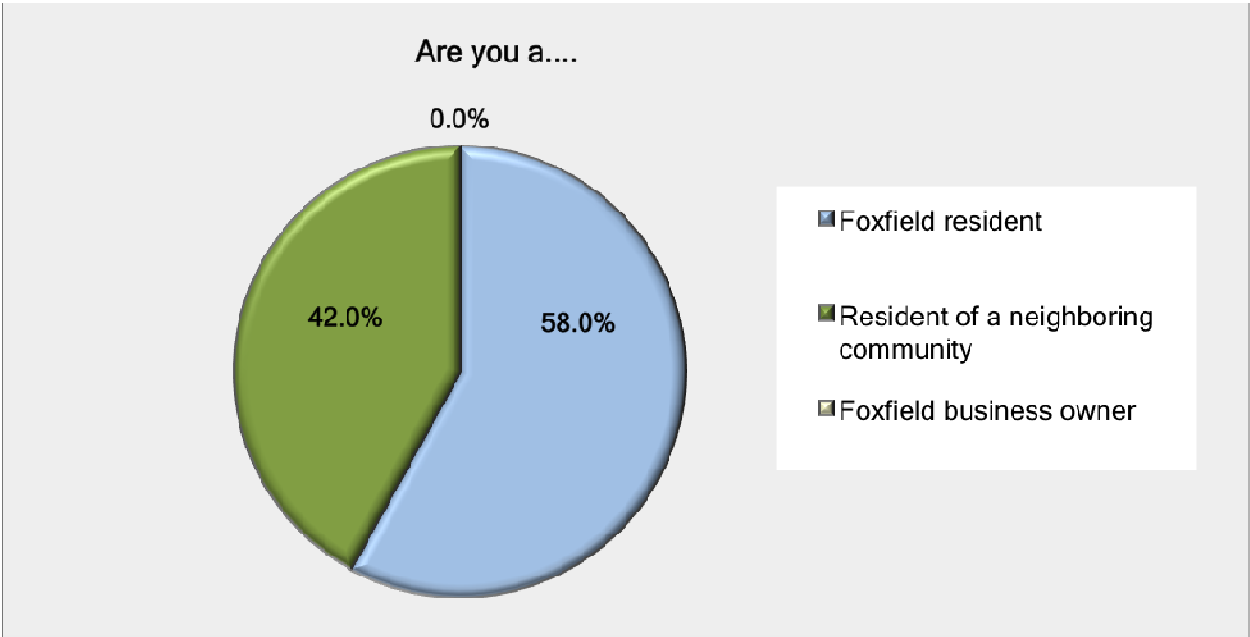
Would like to see the original bridle/walking trails restored before blazing new trails. Also want to be cautious about making it "too inviting" for non residents of Foxfield. We already have problems with cut-through traffic

Responses to Open-Ended Question: "Final Comments – Did we miss anything?"
of vehicles.
Foxfield is a unique rural residential oasis. The ROWs, ditches, setback fences make the town unique. Putting trails/sidewalks in ROWs impractical, expensive, and would destroy Foxfield's unique character. Activities in #2 can be done on roads with increased safely, especially with additional enforcement. Maintenance of trails is expensive. Horse trails may not be compatible with walking trails.
The point of forming Foxfield was to stay rural and not become another City of Aurora. These types of improvements change the rural character of the neighborhood and increase the Town maintenance cost budget. If you want to live in the City, then move there!
If this survey is to ask if we are willing to increase any fees to support more trails, then the ANSWER IS NO!!! But, if you are asking if we see value in quality trail system, then we agree.. I still struggle as to why we are our own Township, because our Taxes are higher & I see no benefit, no amenities, just an entity trying to regulate what we do.. Before I would agree to any more fees, I would have to see value from Town government and so far, all I see is canceled meetings because their value is ZERO!!!
Yes, What will be the estimated cost to maintain the trail system? Each option should have an associated cost for implementing and maintaining.
If the town has the money to spend on trails it should first live up to the promise of paying down the road bond before spending our money on other areas.
Our taxes are already too high. Not willing to pay more.
What do you mean by "support"? No way would I support any tax increase, fee or direct cost to put in trails. Trails also attract people into Foxfield from the Farm. Has anyone considered or looked into the crime impact of this? What about ongoing maintenance costs? How is this funded?
Who would maintain these trails?
Should ask residents about impact to property value.
Bicycles are very dangerous in Foxfield both to cars and pedestrians as they go too fast and don't obey signage.
Why do we need trails. Hardly anyone walks around town. This is a waste of money and time.
DONT DO IT. WE DONT WANT IT
We don't need a trail system.
I do not agree with an "infra-structured" trail way in Foxfield, but would like to see the trail that we do have on county property easement to connect to local businesses and Cherry Creek Trail.
Pedestrian Safety
Walk for 1+ hour daily at CCSP dog off leash area; used to walk in the neighborhood but my dogs need more exercise. An issue: cars do NOT give pedestrians the right-of-way! Most of the time they push past the walkers / joggers even if another car is already passing. All pedestrians should have priority!
I believe that trails that allow for adults and children to ride on/walk on can provide a safer way for them to ride the streets of Foxfield.
We have 9 students that walk to and from Regis Jesuit when the weather allows it. Safe sidewalks are important to this daily commute for these minors.
Perhaps trails would make it safer for children walking to and from the bus stops, which apparently change depending on year.
Miscellaneous Comments
Please put up signs that encourage people to pick up after their dogs.
Put pet stations along the trails.
All trails should be accessible for horse use. Neighbors must continue to keep walk paths mowed. Thanks for the survey!
Building a park or summer recreation center?
We live in Chapparal and feel we should not answer funding questions for Foxfield.
Neighboring Chaparral community has 'open space' that could be converted into a walking trail as well.
The survey is flawed as when 'Other ' is selected, you must still select another answer to let the survey continue.

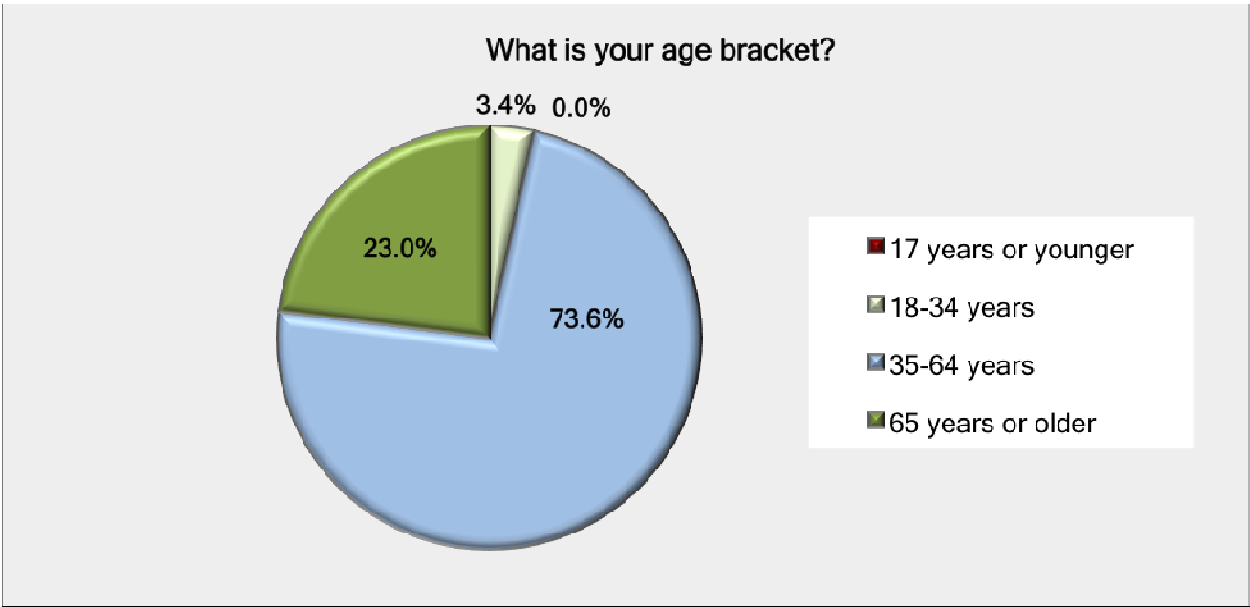


Responses to Open-Ended Question: "Final Comments – Did we miss anything?"
Getting across Parker Road will be quite a challenge...
Foxfield has a wonderful quiet road system! Need access under Parker and Arapahoe roads for access to bike trails and commerce.
It would be of interest to see if the addition of trails could help with the coyote problem that has much of the neighborhood fearful of walking alone or with dogs in the morning and evening.
Future surveys might include links to maps of existing trail network.
Provide the current trail structure in a diagram to send out to everyone then indicate what the new ideas are.
My answer to #2 isn't for walking a pet, but children.

Question 11



Question 12



## **Appendix B.**

### **Community Workshop Summary**

Town of Foxfield  
Trail Plan Community Workshop

***Keypad Polling Results***



**March 2014**

*Prepared for the Town of Foxfield  
by McCool Development Solutions*

## Introduction & Methodology

On March 4, 2014, McCool Development Solutions (MDS) conducted the Foxfield Trail Plan Community Workshop on behalf of the Town of Foxfield, seeking further community engagement as part of the Foxfield Trail Master Plan project. The Master Plan will address the trail and access needs of all Foxfield community members, including pedestrians, cyclists, runners/joggers, and equestrians. The goal of the project is to design a trail network/system in and around the Town of Foxfield that will connect to regional trails such as the Cherry Creek trail system. Ultimately this will provide safer access to nearby retail centers and greater opportunity to enjoy Colorado's great outdoors.

At the March workshop, MDS described the trail planning process, presented results from the Trails Survey administered in January 2014, identified existing conditions, and discussed the opportunities, constraints, and overall goals related to the Trail Plan. The workshop concluded with the opportunity for residents to share their input on potential trail facilities via keypad polling technology. Keypad poll questions were designed by MDS to gather feedback from Foxfield residents on different types of trail facilities and amenities, trail locations, trail construction methods, and other aspects of trail development. Final analysis of all responses is provided below and will provide further input for the Foxfield Trail Master Plan project.

## Summary of Results

The following is a broad summary of the responses to the keypad poll administered at the Foxfield Trail Plan Community Workshop.

Eighteen (18) people responded via the keypad polling technology. The majority of poll respondents (72%) were Foxfield residents. The remaining participants were members of the Trail Advisory group, as well as a few residents from adjacent neighborhoods/HOAs. For many of the questions, participants were shown relevant images and then asked to indicate their level of support, on the following scale: *strongly support, somewhat support, not sure, somewhat oppose, strongly oppose*. Answers that fell within the range of *strongly* to *somewhat support* are combined below to indicate favorable responses (support); answers in the range of *somewhat oppose* to *strongly oppose* are combined below to indicate unfavorable responses (opposition).

Appendix A provides the range of images shown to participants; Appendix B provides maps of trails and proposed routes that were shared with participants. Appendix C provides further detail with a full set of charts and graphs corresponding to the keypad polling questions and responses.



### Trail Facilities

The Planning Team showed images of a range of types of trail facilities to determine which styles participants would find appropriate for Foxfield. Respondents supported the idea of simple trailheads with signage (100%); trail furnishings, such as benches and trashcans (95%); and simple picnic areas alongside trails (56%). The majority (59%) was generally opposed to more elaborate trail facilities, such as landscaped trailheads and dedicated picnic shelter areas. There was little to no support for the idea of an amphitheater as part of the Foxfield parks and trails system.

### Trail Development

Overall, respondents supported the concept of trail development in the following locations: along existing fenced utility corridors (88%), existing road corridors (61%), and within Foxfield's parks and open space areas, such as Norfolk Open Space (100%).

### Parkland and Open Space

Respondents generally supported the concept of more parkland and open space in Foxfield (63%).

### Road Corridors

Respondents were next shown maps of proposed trail routes in Foxfield, as well as images of types of trails, and asked a series of questions about specific roads in town: Richfield Street, Easter Avenue, and the Arapahoe Road Corridor.

Overall, respondents supported the idea of at the very least an unpaved trail corridor along both Richfield Street (64%) and Easter Avenue (95%). Respondents were fairly divided in their support for/opposition to paved trail corridors, and for bike routes or bike lanes in these locations. Of these two locations, more respondents (39%) would support a bike route along Richfield Street than along Easter Avenue. A large majority of respondents (89%) were in support of completing the missing sidewalks links along the Arapahoe Road Corridor.

Respondents were asked questions regarding support for an internal, roadside loop trail within the public right-of-way, linking to primary trail corridors. They generally supported the idea of an unpaved loop trail (78%). A majority (78%) was opposed to the idea of a bike route or a bike lane along the loop trail.

### Norfolk Multi-use Trail

All respondents (100%) supported to some extent the completion of the Norfolk Multi-use Trail (west of Ring Road).

### Drainage Channel Trail

A majority of respondents (61%) generally supported the idea of a trail or open space corridor along the existing drainage way. A majority (58%) also indicated that they supported the idea of the Town working with CDOT and the City of Centennial to provide a grade separated crossing at Parker Road along the drainage channel.

### Trail Easements

The keypad polling next focused on questions related to trail easements for a northern trail (connecting Norfolk Trail with Loop Trail and S. Richfield Trail) and southern trail (along existing utility corridors), with the final alignment in both cases subject to easements provided.

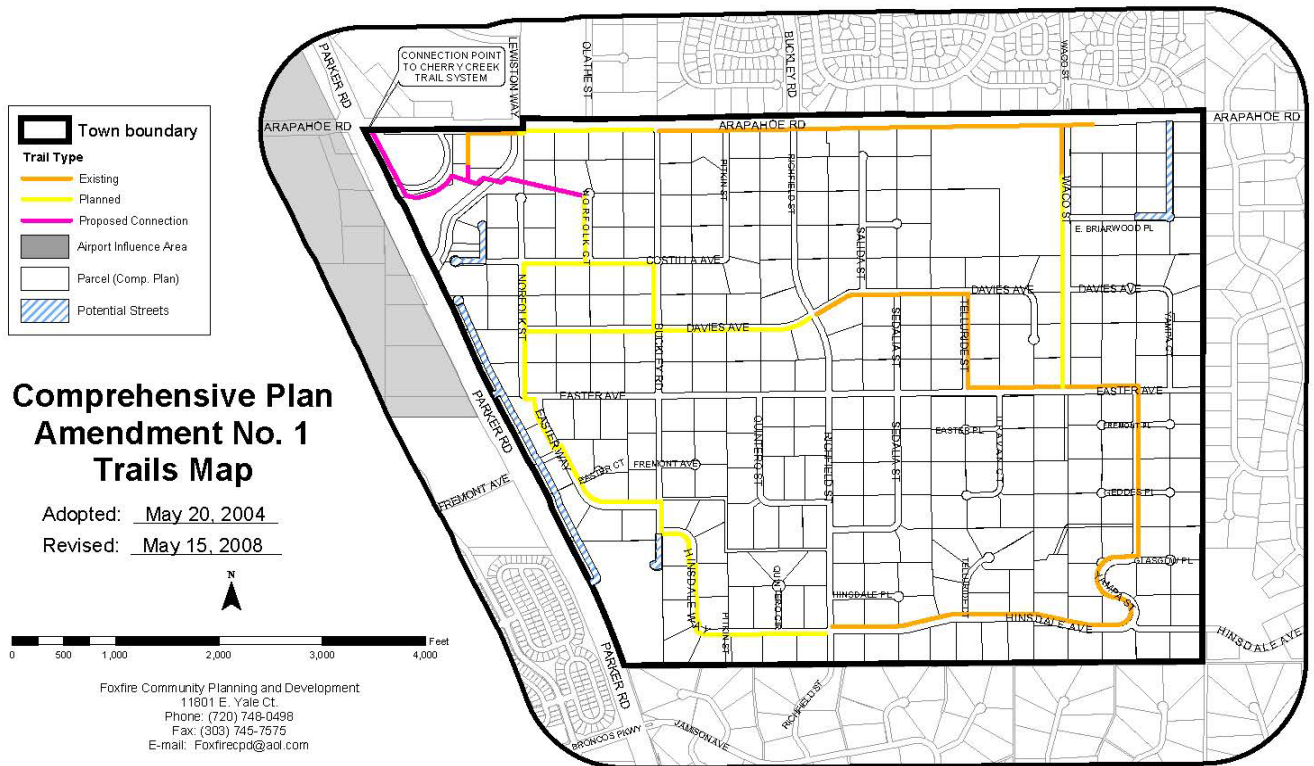
Roughly 50% of poll respondents would support a natural/unpaved northern trail link if property owners were willing to provide trail easements; roughly 88% would support the same idea in the southern location.

### Proposed Trail Cross Sections

Finally, the Planning Team presented the workshop attendees with a series of drawings showing proposed cross-sections for trails in the public right-of-way, multi-use or open space trails, non-roadway trail easements, and bike routes. The majority of respondents (78%) indicated general support for the shown cross-sections.

*Trail Maps, Proposed Routes,  
& Cross Sections*

## 2008 Town of Foxfield Trail Plan Map

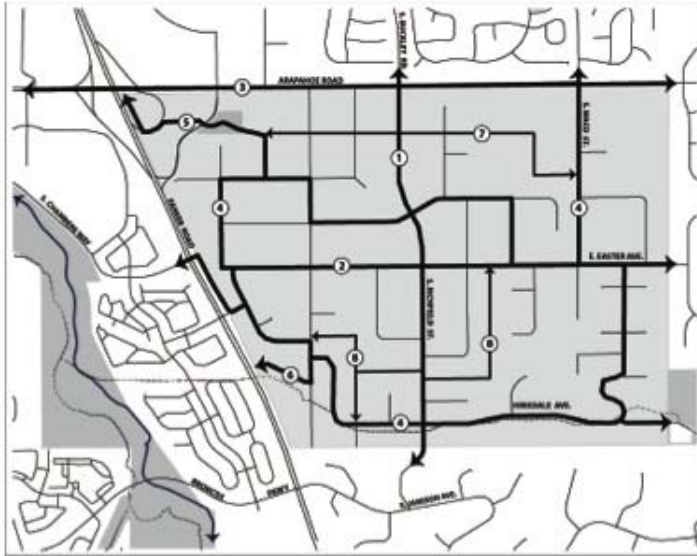


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## Maps for Keypad Questions #13-28



## Proposed Trail Routes



1. S. Richfield Street Trail
2. E. Easter Avenue Trail
3. Arapahoe Road Sidewalk
4. Loop Trail
5. Norfolk Open Space Trail
6. Drainage Corridor Trail
7. Northern Trail Easement
8. Southern Trail Easement

## 1. South Richfield Trail Corridor

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- Roadside Trail/Bike Route
- Primary north/south trail corridor
- Linkage to Piney Creek Trail via signalized intersection (Arapahoe Rd)
- Linkage to Chenango Neighborhood (south)
- Located within public right of way



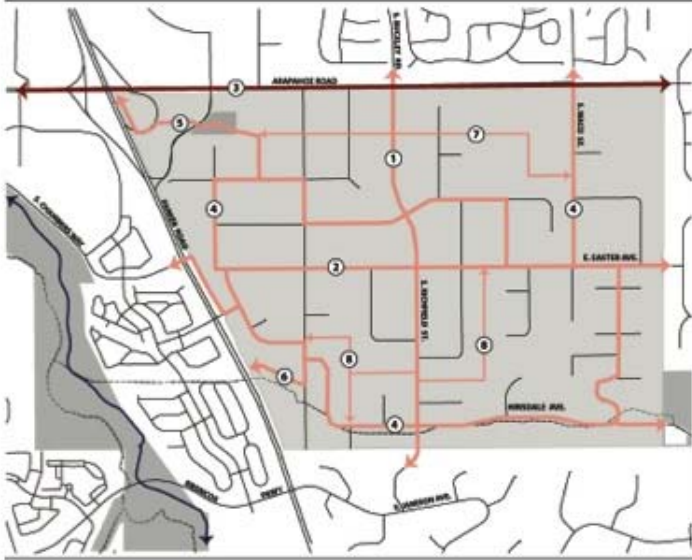
## 2. East Easter Trail Corridor



- Roadside Trail/Bike Route
- Primary east/west trail corridor
- Linkage to Chapparal Neighborhood (east)
- Linkage to Cherry Creek Trail via signalized intersection (west)
- Located within public right of way



### 3. Arapahoe Road Corridor

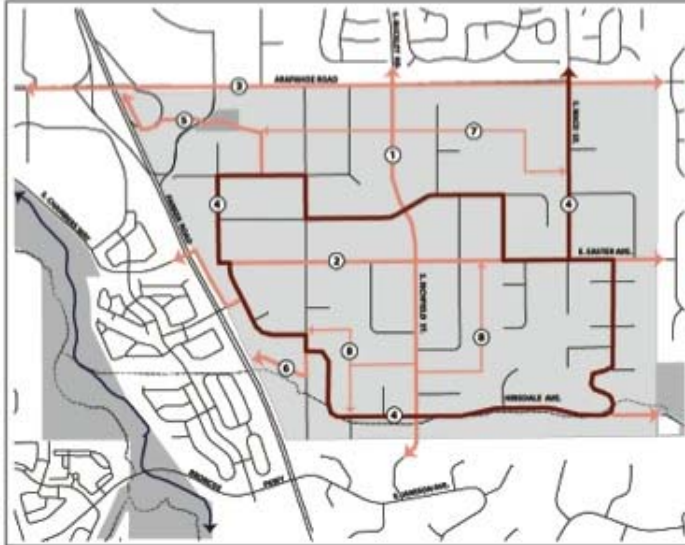


- East/West Sidewalk Connector
- Linkage to Cherry Creek Regional Trail (west)
- Allows pedestrian access to Retail Centers along Arapahoe Road (Corner Star & Foxfield Village Center)
- Provides Access to Regis High School & Fox Hollow Elem





## 4. Loop Trail



- Roadside Trail
- Internal Loop Trail linking to Primary Trail Corridors
- Linkage to Fox Hollow School (north) via signalized intersection at Waco St.
- Located within public right of way



## 5. Norfolk Open Space



- Multi-Use Trail/Open Space Trail
- Linkage to Cherry Creek Regional Trail via Arapahoe Road
- Two existing below grade road crossings (Ring Road & Loop Road)
- Partially located within existing Town owned open space
- Trail easement required along private property



## 6. Drainage Corridor Trail



- Multi-Use/Open Space Trail
- Potential link to existing trail west of Parker Road
- Below grade crossing required under Parker Road
- Trail easement required along private property





## 7. Northern Trail Easement



- Natural/Unpaved Trail
- Non-roadway trail easement connecting Norfolk Trail with Loop Trail and S. Richfield Trail
- Trail easements required along private property
- Final alignment may vary depending on where easements are provided



## 8. Southern Trail Easement



- Natural/Unpaved Trail
- Trail along existing utility corridors
- Trail easement required along private property
- Final alignment may vary depending on where easements are provided
- Some existing fences already define a trail corridor

### Proposed Trail Cross Sections for Keypad Question #31

#### Public ROW Trail (paved/unpaved)



- Paved or Unpaved Trail
- Generally located along fence/ROW line
- Additional trail easements may be necessary in some locations
- Trail Corridors: South Richfield, East Easter, Loop Trail

## Multi-Use/Open Space



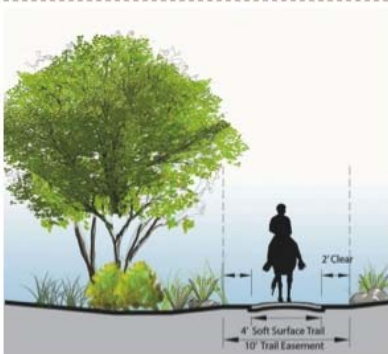
- Paved Trail (adjacent soft surface trail)
- Generally located in parks, open spaces
- Trail easements required on private property
- Trail Corridors: Norfolk Trail and I-635

## Bike Route



- All public roadways provide access for experienced cyclist
- Shared Paved Roadway or Dedicated Bike Lane
- Bike routes designated by signage along roadway

## Non-roadway Trail Easements



- Unpaved Trail
- Generally located on private property along utility corridors
- Trail easements required
- Trail Corridors: Northern Trail Easement and Southern Trail Easement



*Charts and Images for  
Keypad Polling Responses*

## Graphic Keypad Polling Results by Question

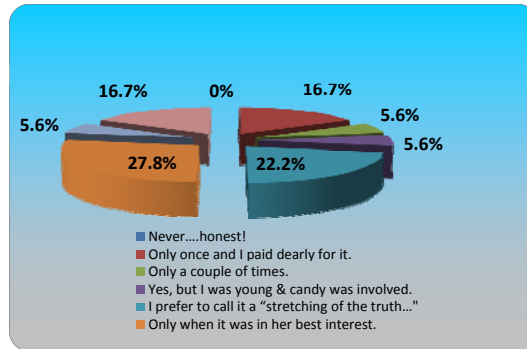
Session Name: Foxfield 3-4-2014 8-16 PM

Created: 3/14/2014 11:29 AM

## 1.) Have you ever lied to your mother?

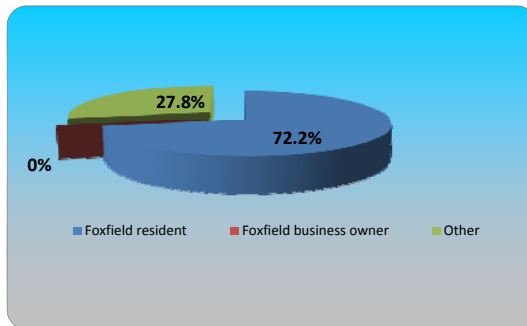
(multiple choice)

Never....honest!	0%
Only once and I paid dearly for it.	16.7%
Only a couple of times.	5.6%
Yes, but I was young & candy was involved.	5.6%
I prefer to call it a "stretching of the truth..."	22.2%
Only when it was in her best interest.	27.8%
Yes, but my brother/sister made me do it.	5.6%
Too many times to count.	16.7%
<b>Totals</b>	<b>100%</b>

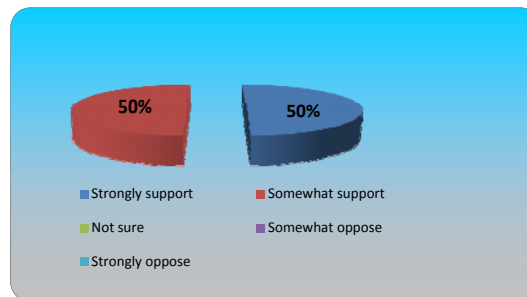


## 2.) Are you a . . . (multiple choice)

Foxfield resident	72.2%
Foxfield business owner	0%
Other	27.8%
<b>Totals</b>	<b>100%</b>

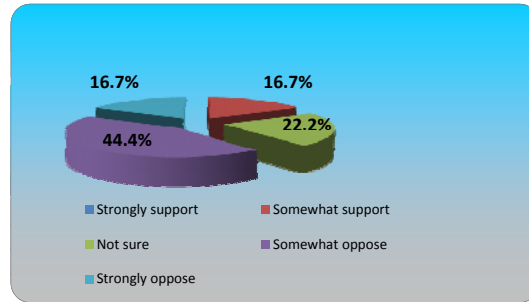


Strongly support	50%
Somewhat support	50%
Not sure	0%
Somewhat oppose	0%
Strongly oppose	0%
<b>Totals</b>	<b>100%</b>



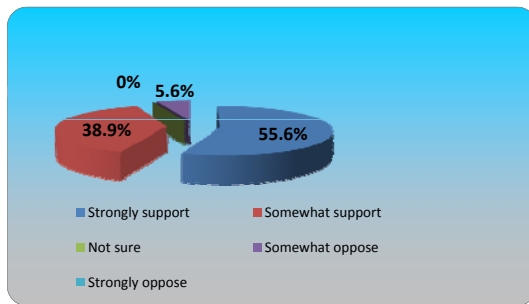
**4.) Is this trail facility appropriate for Foxfield? (multiple choice)**

Strongly support	0%
Somewhat support	16.7%
Not sure	22.2%
Somewhat oppose	44.4%
Strongly oppose	16.7%
<b>Totals</b>	<b>100%</b>



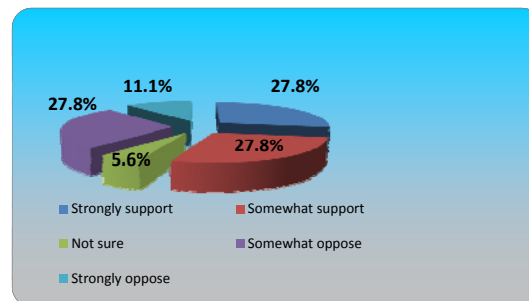
**5.) Is this trail facility appropriate for Foxfield? (multiple choice)**

Strongly support	55.6%
Somewhat support	38.9%
Not sure	0%
Somewhat oppose	5.6%
Strongly oppose	0%
<b>Totals</b>	<b>100%</b>



**6.) Is this trail/park facility appropriate for Foxfield? (multiple choice)**

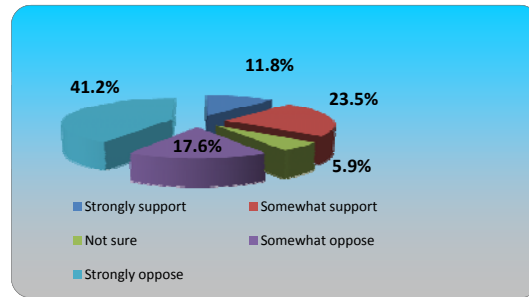
Strongly support	27.8%
Somewhat support	27.8%
Not sure	5.6%
Somewhat oppose	27.8%
Strongly oppose	11.1%
<b>Totals</b>	<b>100%</b>





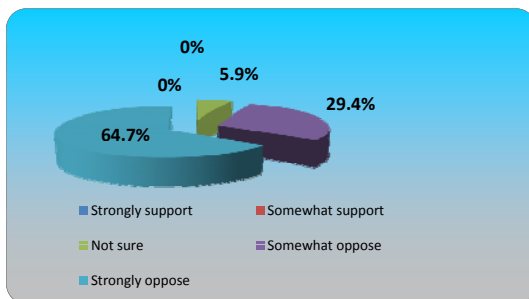
**7.) Is this trail/park facility appropriate for Foxfield? (multiple choice)**

Strongly support	11.8%
Somewhat support	23.5%
Not sure	5.9%
Somewhat oppose	17.7%
Strongly oppose	41.2%
<b>Totals</b>	<b>100%</b>



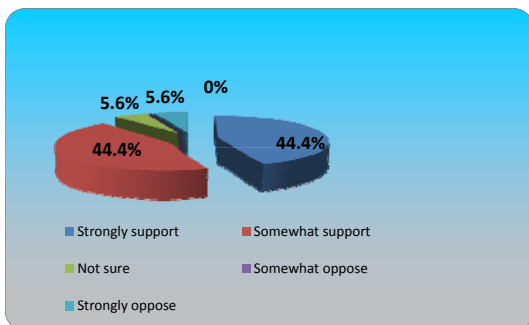
**8.) Is this trail/park facility appropriate for Foxfield? (multiple choice)**

Strongly support	0%
Somewhat support	0%
Not sure	5.9%
Somewhat oppose	29.4%
Strongly oppose	65%
<b>Totals</b>	<b>100%</b>



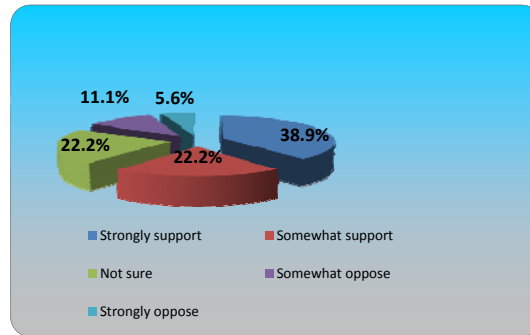
**9.) Do you support trail development along existing fenced utility corridors in Foxfield? (multiple choice)**

Strongly support	44.4%
Somewhat support	44.4%
Not sure	5.6%
Somewhat oppose	0%
Strongly oppose	6%
<b>Totals</b>	<b>100%</b>



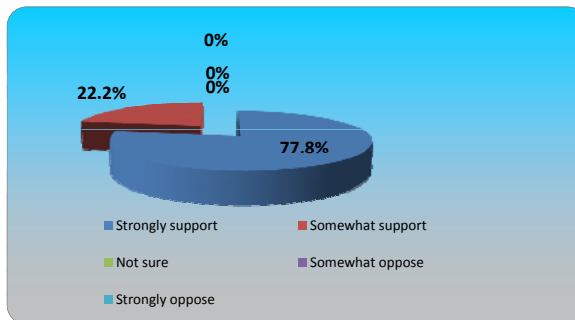
**10.) Do you support trail development along existing road corridors? (multiple choice)**

Strongly support	38.9%
Somewhat support	22.2%
Not sure	22.2%
Somewhat oppose	11.1%
Strongly oppose	6%
<b>Totals</b>	<b>100%</b>



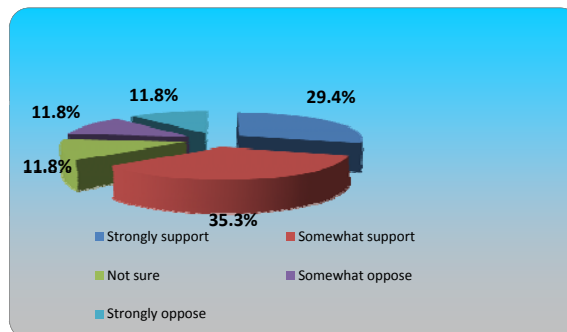
**11.) Do you support trail development in Foxfield's Park & Open Space areas (multiple choice)**

Strongly support	77.8%
Somewhat support	22.2%
Not sure	0%
Somewhat oppose	0%
Strongly oppose	0%
<b>Totals</b>	<b>100%</b>



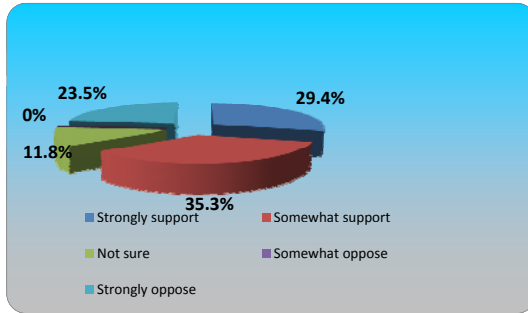
**12.) Would you like to see more parkland and open space in Foxfield? (multiple choice)**

Strongly support	29.4%
Somewhat support	35.3%
Not sure	11.8%
Somewhat oppose	11.8%
Strongly oppose	11.8%
<b>Totals</b>	<b>100%</b>



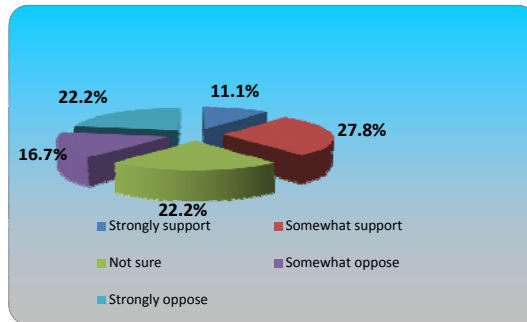
**13.) Do you support the idea of a trail corridor along Richfield Street? (multiple choice)**

Strongly support	29.4%
Somewhat support	35.3%
Not sure	11.8%
Somewhat oppose	0%
Strongly oppose	23.5%
<b>Totals</b>	<b>100%</b>



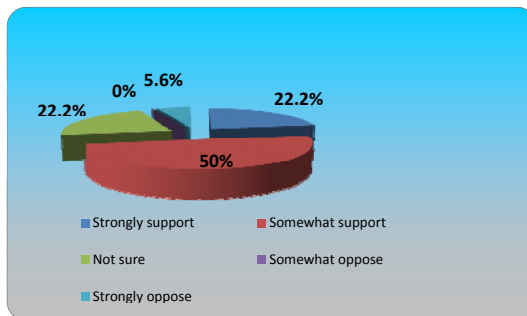
**14.) Do you support a paved trail along Richfield Street? (multiple choice)**

Strongly support	11.1%
Somewhat support	27.8%
Not sure	22.2%
Somewhat oppose	16.7%
Strongly oppose	22.2%
<b>Totals</b>	<b>100%</b>



**15.) Do you support an unpaved roadside trail along Richfield Street? (multiple choice)**

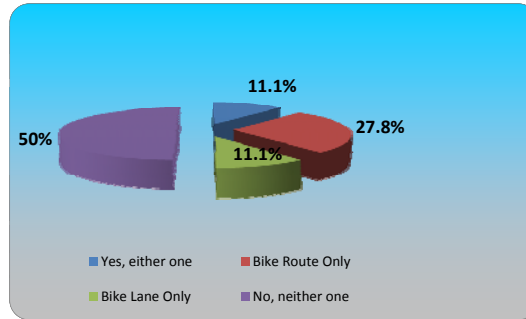
Strongly support	22.2%
Somewhat support	50%
Not sure	22.2%
Somewhat oppose	0%
Strongly oppose	5.6%
<b>Totals</b>	<b>100%</b>





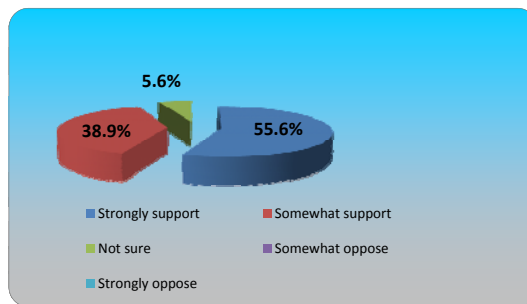
**16.) Do you support bike route or bike lane along Richfield Street? (multiple choice)**

Yes, either one	11.1%
Bike Route Only	27.8%
Bike Lane Only	11.1%
No, neither one	50%
<b>Totals</b>	<b>100%</b>



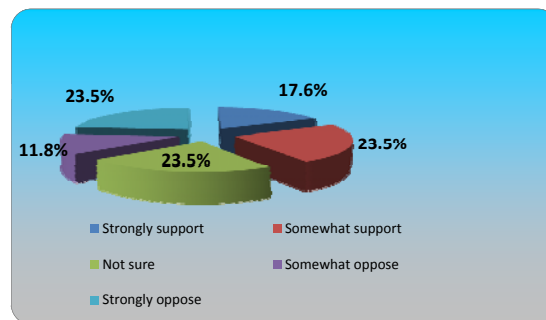
**17.) Do you support the idea of a trail corridor along Easter Ave? (multiple choice)**

Strongly support	55.6%
Somewhat support	38.9%
Not sure	5.6%
Somewhat oppose	0%
Strongly oppose	0%
<b>Totals</b>	<b>100%</b>



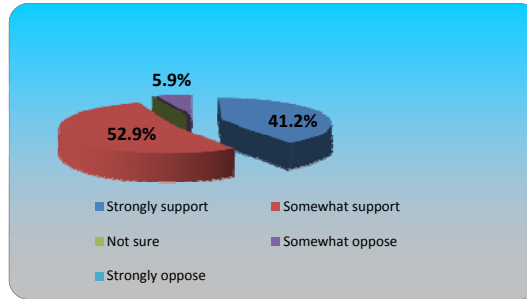
**18.) Do you support a paved trail along Easter Ave? (multiple choice)**

Strongly support	17.7%
Somewhat support	23.5%
Not sure	23.5%
Somewhat oppose	11.8%
Strongly oppose	23.5%
<b>Totals</b>	<b>100%</b>



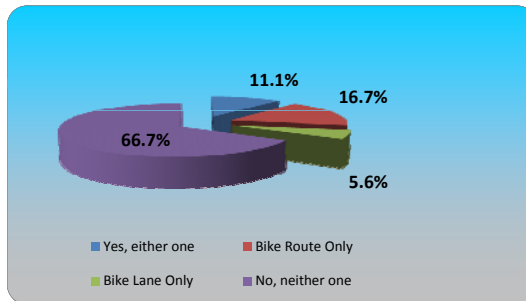
**19.) Do you support an unpaved trail along Easter Ave? (multiple choice)**

Strongly support	41.2%
Somewhat support	52.9%
Not sure	0%
Somewhat oppose	5.9%
Strongly oppose	0%
<b>Totals</b>	<b>100%</b>



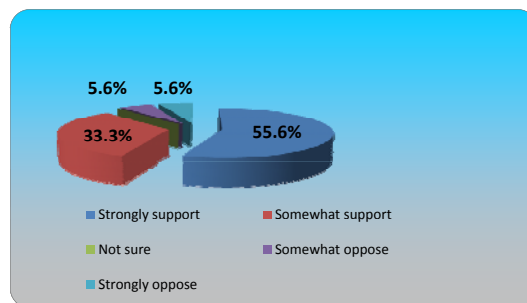
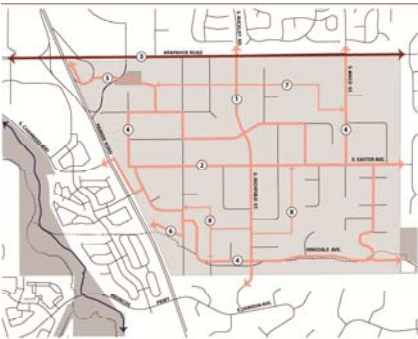
**20.) Do you support bike route or bike lane along Easter Ave? (multiple choice)**

Yes, either one	11.1%
Bike Route Only	16.7%
Bike Lane Only	5.6%
No, neither one	66.7%
<b>Totals</b>	<b>100%</b>



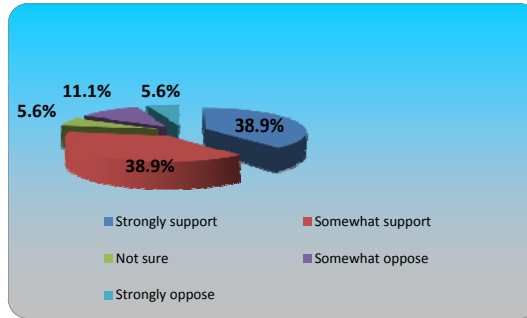
**21.) Do you support completing missing sidewalk links along the Arapahoe Road Corridor? (multiple choice)**

Strongly support	55.6%
Somewhat support	33.3%
Not sure	0%
Somewhat oppose	5.6%
Strongly oppose	5.6%
<b>Totals</b>	<b>100%</b>



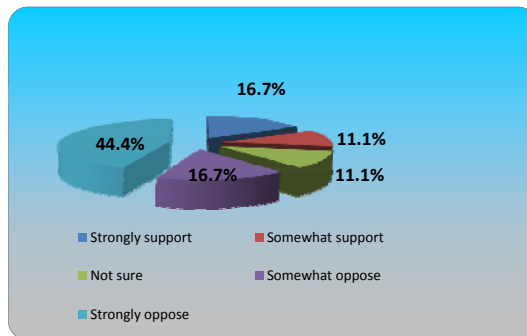
**22.) Do you support the idea of a loop trail corridor? (multiple choice)**

Strongly support	38.9%
Somewhat support	38.9%
Not sure	5.6%
Somewhat oppose	11.1%
Strongly oppose	5.6%
<b>Totals</b>	<b>100%</b>



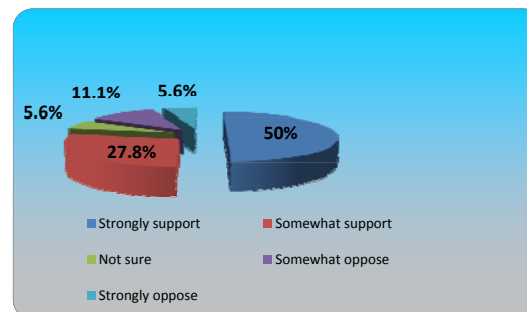
**23.) Do you support a paved loop trail? (multiple choice)**

Strongly support	16.7%
Somewhat support	11.1%
Not sure	11.1%
Somewhat oppose	16.7%
Strongly oppose	44.4%
<b>Totals</b>	<b>100%</b>



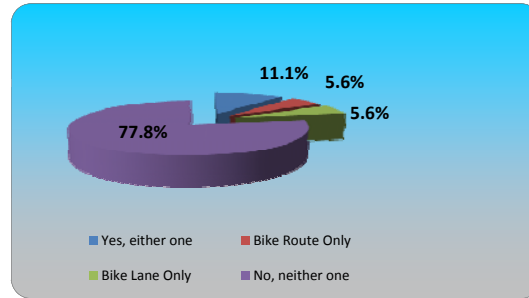
**24.) Do you support an unpaved loop trail? (multiple choice)**

Strongly support	50%
Somewhat support	27.8%
Not sure	5.6%
Somewhat oppose	11.1%
Strongly oppose	5.6%
<b>Totals</b>	<b>100%</b>



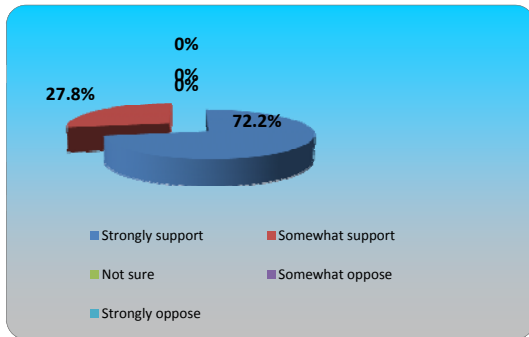
**25.) Do you support bike route or bike lane?  
(multiple choice)**

Yes, either one	11.1%
Bike Route Only	5.6%
Bike Lane Only	5.6%
No, neither one	77.8%
<b>Totals</b>	<b>100%</b>



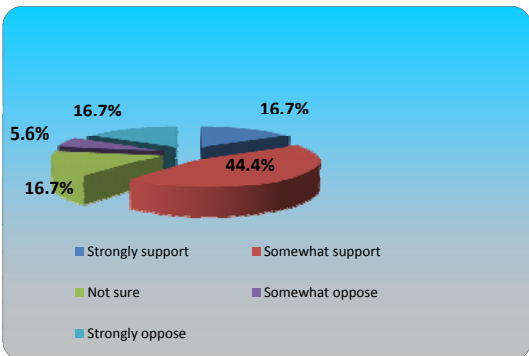
**26.) Do you support the completion of the  
Norfolk Multi-use trail (west of Ring Road)?  
(multiple choice)**

Strongly support	72.2%
Somewhat support	27.8%
Not sure	0%
Somewhat oppose	0%
Strongly oppose	0%
<b>Totals</b>	<b>100%</b>



**27.) Do you support the idea of a trail or open  
space corridor along the existing drainage  
way? (multiple choice)**

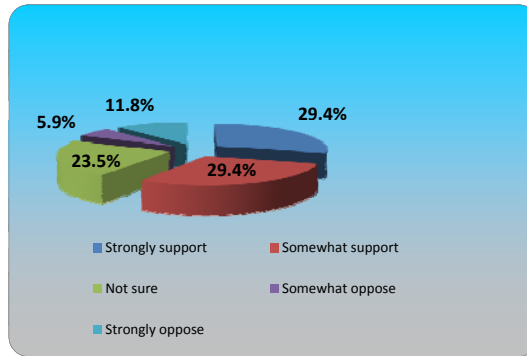
Strongly support	16.7%
Somewhat support	44.4%
Not sure	16.7%
Somewhat oppose	5.6%
Strongly oppose	16.7%
<b>Totals</b>	<b>100%</b>





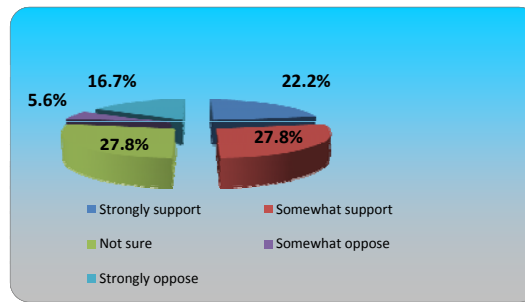
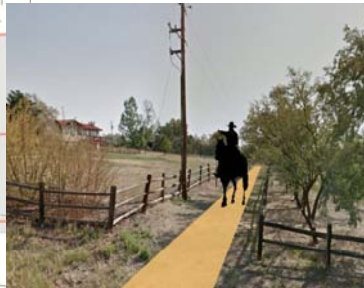
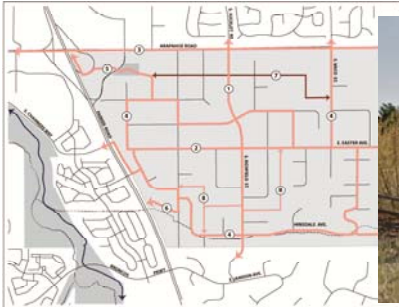
**28.) Should the Town work with CDOT and the City of Centennial to provide a grade separated crossing at Parker Road along the drainage channel? (multiple choice)**

Strongly support	29.4%
Somewhat support	29.4%
Not sure	23.5%
Somewhat oppose	5.9%
Strongly oppose	11.8%
<b>Totals</b>	<b>100%</b>



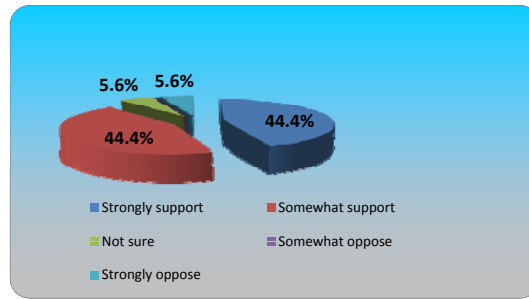
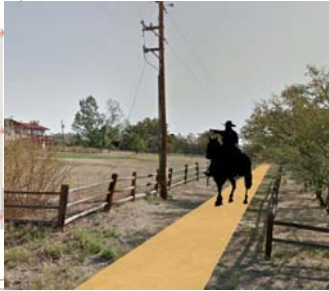
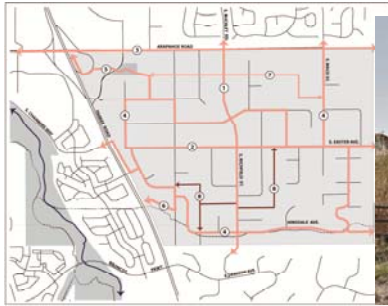
**29.) If property owners are willing to provide trail easements, would you support a natural/unpaved trail link? (multiple choice)**

Strongly support	22.2%
Somewhat support	27.8%
Not sure	27.8%
Somewhat oppose	5.6%
Strongly oppose	16.7%
<b>Totals</b>	<b>100%</b>



**30.) If property owners are willing to provide trail easements, would you support a natural trail in this location? (multiple choice)**

Strongly support	44.4%
Somewhat support	44.4%
Not sure	5.6%
Somewhat oppose	0%
Strongly oppose	5.6%
<b>Totals</b>	<b>100%</b>



**31.) Do you support (in general) the presented trail cross sections? (multiple choice)**

Strongly support	38.9%
Somewhat support	38.9%
Not sure	22.2%
Somewhat oppose	0%
Strongly oppose	0%
<b>Totals</b>	<b>100%</b>

